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# Ports and Harbours of Gwynedd: **Aberdyfi**

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## **A Threat Related Assessment**

GAT Project No. 1824

Report No. 671.1

April, 2007



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Report No. 671.1

Prepared for CADW

April 2007

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# Ports and Harbours of Gwynedd: **Aberdyfi**

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# Ports and Harbours of Gwynedd: **Aberdyfi**

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## **ABERDYFI**

### **1. INTRODUCTION**

#### **1.1 General Introduction**

This report contains the results of an archaeological assessment undertaken as part of a wider assessment of ports and harbours throughout Wales. The assessments have been undertaken by the Welsh Archaeological Trusts and funded by CADW. The report has been compiled by the Gwynedd Archaeological Trust.

#### **1.2 Aims of the project**

The aim of the project is to identify the nature and status of the heritage resource within the study area and to aid and encourage the incorporation of the resource into the management and future development of the coastal zone.

#### **1.3 Location**

The port and village of Aberdyfi (SH 615960) are situated on the northern shore of the Dyfi estuary, at the southern littoral tip of Gwynedd. Behind them rises a ridge of hills rising to 278m above ordnance datum. The shoreline has seen significant levels of change since the 18<sup>th</sup> century; Lewis Morris' manuscript map of 1741-5 emphasises the way in which the 18<sup>th</sup> century shoreline around Aberdyfi is far more indented than the present one. A series of deeper pools on the Dyfi means that it is possible for ships of fairly large tonnage to make their way to near the tidal limit at Derwenlas, within reach of Machynlleth (Lloyd 1996, 8).

There is no north-south route surviving, but it seems probable that, before the construction of the first coast road in 1808 by the Ynysmaengwyn estate the principle route in and out was north *via* Copperhill Street and over the high ground to Cwm Maethlong (Happy Valley) and then west to Tywyn or east to Pennal (this is discussed further in 2.4.2 below). The settlement developed within the sheltered confines of the narrow valley, whilst to the east a parallel valley was occupied by the small settlement of Penhelyg. Along the sea front only a small narrow terrace was available for building, and the majority of this was not developed until the 19<sup>th</sup> century when the tall 3-storey terraces were built to fuel the rise in popularity of Aberdyfi as a holiday resort. The improvement in roads and the construction of the railway resulted in a pronounced east-west orientation of the settlement, so that Aberdyfi and Penhelyg became linked by a continuous run of buildings.

### **2. HISTORICAL BACKGROUND**

#### **2.1 Introduction**

The port of Aberdyfi has fulfilled many functions. Historically, like many others in Gwynedd, it drew its chief revenue from mineral exports, but it was not a mineral sump in the way of Port Penrhyn, y Felinheli or Porth Amlwch. Like Conwy, Caernarfon, Porthmadog and Pwllheli, it was a sub-regional *entrepot*, where domestic goods, foodstuffs and cattle were landed alongside exports. Like Conwy, Caernarfon and Porthmadog, the complexity of transport within its immediate hinterland means that its broader historic environment includes a wealth of inter-modal transport links – from road and rail to navigable water, rail to rail and ship to boat. Like Conwy and Barmouth, it lies at the mouth of a tidal inlet which gave access to rural markets in which a borough town lay just above the tidal head. However, the mouth of the Dyfi is in some respects strategically more significant than the Conwy or the Mawddach. It is one of the 'fulcrum points' of Wales, the point where north meets south, and where Wales itself, measured east to west, is at its narrowest, the 'wasp waist' of the country. Its central location underlies the legend of the chair of Maelgwn Gwynedd, and the election of a monarch over the British people. Like Porthmadog and Barmouth, Aberdyfi was also important for its location along the north-south route along Cardigan Bay, and like Barmouth, Crickieth and Pwllheli, it also became important early on as a holiday resort.

It is scarcely surprising that the mouth of the Dyfi should have yielded its traditions and tales. In many ways, the Dyfi was a nodal point of medieval Wales, and both the ferry-crossing itself and the landscape to its south would have left a strong impression on anyone who had to make the journey. Several tales refer to a drowned kingdom in Cardigan Bay, described in the Latin *Chronica de Wallia* as:

the ... kingdom of Helig son of Glannog ... between Ceredigion and Bardsey, and as far as St David's. That land was very good, fertile and level, and it was called Maes Maichgen; it lay from the mouth (of the Ystwyth?) to Llŷn, and upwards to Aberdovey (Bromwich 2006, lxxv).

Legends of drowned kingdoms are met in all the countries on the western sea-board of Europe, and surely represent a folk-tale motif rather than any real memory of a time when the land extended into Cardigan Bay. Yet the tradition has certainly had a resonance in Wales, and underlay the anti-Tory satire of Thomas Love Peacock in the early nineteenth century, as well as giving rise to Ceiriog's sentimental ballad, 'Clychau Aberdyfi' ('The Bells of Aberdovey'), that classic of late Victorian parlour entertainment song-book.

Lewis Morris was collector of the customs at Aberdyfi from 1752 to 1756 (Wiliam, *DNB*).

## **2.2 Historic administration**

The settlement at Aberdyfi is located within the historic parish of Tywyn, and the commote of Ystumanner in the county of Merioneth. The commotal centre was at Pennal, where a motte is situated (SH 6970 0024). Pennal retained some administrative functions into the 19<sup>th</sup> century, sharing with Aberdyfi the petty sessions, held once every two months (Lewis 1838, C2v). It is possible that Pennal itself was the scene of the assembly held by Llywelyn ab Iorwerth in 1216 known as the assembly of Aberdyfi (*DWB*). Pennal was certainly the court from which Glyndwr wrote to King of France in 1406 (Davies 1995, 162, 169).

Within the period of documented history Aberdyfi was a proprietary harbour which formed part of the Ynysmaengwyn estate until it was leased by the Aberystwith (sic) and Welsh Coast Railway and passed on to their successors, the Cambrian Railways, the Great Western Railway and British Railways. The Crown had rights of foreshore. With the extension of the English customs system into Wales in the mid-16<sup>th</sup> century, Aberdyfi became a port of Milford. Thereafter it was the principal custom port for Cardigan Bay until 1770, when administration was passed to Aberystwyth. From 1786 Aberdyfi became a creek (sub-port) of Aberystwyth (Lloyd 1996, 6-7, 45, 61).

## **2.3 Enclosure**

Although none of the land in the immediate area of Aberdyfi was enclosed in the 18<sup>th</sup> or 19<sup>th</sup> century, considerable tracts of land in its hinterland and (more particularly) to the south of the estuary were subject to Parliamentary enclosure. These projects have historically been overshadowed by Madocks' attempts to build the Traeth Mawr cob in Gwynedd, but should be seen as part of a pattern of building sea-defences and harbours which went on along the west coast of Wales from Malltraeth in Anglesey to Aberaeron in Ceredigion, and which transformed the coastline in the late 18<sup>th</sup> and early 19<sup>th</sup> centuries.

The Corbets enclosed and drained Tywyn marsh from 1818 onwards (Chapman 1992, 111-2) though a far bigger project was the enclosure of Gors Fochno on the southern bank of the Dyfi in the parishes of Llangynfelyn and Llanfihangel Genau'r Glyn. This was carried out under an act of 1813 and the award made in 1847, and affected a total of 10,000 acres. The effect this had on the area to the south of Aberdyfi was profound; the Afon Lenni was diverted from its old course and instead of reaching the sea at Aberlenni, thereafter flowed along a straight newly-cut course to the Dyfi at Ynyslas (Chapman 1992, 54-5, Colyer 1977, 181-192).

## **2.4 Transport**

### *2.4.1 Ferry*

The presence of a Roman fort and of a *llys* at Pennal suggests that at one time the focus for crossing the Dyfi was higher up the river than the present town of Aberdyfi, in the region of SN 7031 9960. The first documentary evidence for a ferry across the mouth of the river comes in the writings of Giraldus Cambrensis, who records that on Thursday 7 April 1188 he and Archbishop Baldwin made their way from Llanbadarn Fawr to Tywyn; Gerald's account states baldly 'We crossed the river in boats and so left the diocese of St David's. Now we were in Merioneth, the territory of the sons of Cynan, the first province of Gwynedd to those coming from that direction and part of the diocese of Bangor' (Gerald 1976, 182).

Many Aberdyfi mariners came from Borth and the ferry across the estuary remained an important artery of communication for local people until the 1940s (Morgan 1996, xxii).

#### 2.4.2 Roads

Given the pattern of settlement in the town of Aberdyfi itself, with attested houses of eighteenth century date surviving at SH 6138 9603, it seems likely that a pre-Modern road connecting with the ferry followed the route of the present Copperhill Street and climbed up Bwlchwyn to Cwm Maethlon and thereby to Tywyn. However, the east-west turnpike built following the Merioneth turnpike act of 1775 by-passed Aberdyfi entirely, running from Pennal through Cwm Maethlon to Tywyn (Morgan 1948, 91).

If the pre-Modern settlement of Aberdyfi is based around a north-south figuration, the effect of the 'Improvements' and other technical changes of the late 18<sup>th</sup> century onwards was to make the principal configuration an east-west one. Fenton remarks in 1808 that a new road was under construction from Aberdyfi to Machynlleth but implies that the section from Pennal to Machynlleth was already in being, that an extension west and north to Tywyn 'by way of the sands' was contemplated if not actually under construction (Fenton 121). This road, known as 'hen ffordd Corbet' was not a success, being built so low that it was frequently covered by the tides (Morgan 92). Its course is marked on the plans for the replacement road dated 1823 and prepared by Thomas Penson (DRO: Z/CD/168). This is probably Thomas Penson junior (1790-1859), county surveyor of Montgomeryshire, a versatile and able architect-engineer, rather than his father (c. 1760-1824 – Skempton 2002, 518-9). Lewis states that this road was completed in 1827 and adds:

For nearly the whole of its extent from Pennal to Aberdovey, it is cut through the solid rock, which, in many places, presents its naked side, of different elevations, forming a pleasing contrast to the wooded declivity of the hill ... (Lewis 1838, C2r).

The building of the road not only altered the pattern of settlement but also led to the construction of hotels to cater for visitors. These are noted separately below.

#### 2.4.3 Railway

The building of the railway as originally planned would have restored the north-south axis, in that Thomas Savin, the promoter of the Aberystwith (sic) and Welsh Coast Railway, originally intended the branch from the main Oswestry-Aberystwyth line to cross the Dyfi on a viaduct similar to the one erected at Barmouth. An isolated section of track operated between Aberdyfi Harbour and Llwyngwriil from October 1863. Savin's failure interrupted the completion of the Aberdyfi to Dovey Junction section until August 1867, after which passenger services ceased to use the harbour branch. The short section from the main Aberdyfi station (named Aberdovey [Junction] for many years) continued to be used by goods trains until 4 May 1964 to the wharf (see below). The track was subsequently lifted (Bradley 1992, 104).

The principal station, Aberdovey (Junction), lies some way to the west of the town, reflecting the constricted space which had obliged to the engineer of the line from Dyfi Junction to tunnel behind the town for much of the way. Easy rail access through Shrewsbury meant that the railway confirmed what the road system had already made possible, that Aberdyfi should develop much like Aberystwyth and Barmouth, as a resort for the middle classes of the English midlands. It also meant that it could continue to function as an industrial port, with the benefit of short-haul rail

traffic from production sites in the immediate area, and that it could develop as an inward port, with better forward transport links. When the railway system at Aberdyfi became part of the Great Western railway empire in 1922-3, the harbour became part of the largest single port-owning organisation in the world.

The canopy was removed in the late 20<sup>th</sup> century for re-erection at Llanuwchllyn Station on the Bala Lake Railway (pers. comm., Roger Hine).

#### 2.4.4 Wharfage and quays

There is evidence that the landing area was equipped with storehouses as early as the 17<sup>th</sup> century even though it was probably innocent of any built-up quays at that stage. The will of William Vaughan dated 1677 refers to:

All that messuage houses and garden messuage houses storehouses and gardens in Aberdyfi now or late in the severall & respective tenures of David Richard, William John Griffith, Lewis Morgan, Mr William Peterson, Resse Rowland, Thomas David, Peter William Rees, Mr Robert Hayward, Mr Edward Hughes, and my *so(lacuna)* all that great storehouse called Ty Llwyd, the lime kill (*sic*) and lime storehouse .... Keys and all other my storehouses rights, priviledges, profits and inheritances lying between the brookes of Penhelig & Aberdyfi (NLW Peniarth DA383).

This clearly suggests a remarkably high level of economic activity in Aberdyfi in this period. However, there is little other clear documentary evidence, or visible archaeological evidence for wharfage or quays before the coming of the railway. From 1828 the trade directories refer to the 'quay' at Aberdyfi, implying an engineered structure of some description, and it is interesting that at this date a distinction appears to be made between the Corbet Arms Hotel 'on the Beach' and the immediately adjacent 'Royal Raven ... Quay Aberdovey'. Possibly the 'quay' was simply a built-up section of the road from Pennal to Tywyn. The known export of mineral ores would suggest that bins for storage might have been required but no visual or material record has been established, and it is possible that they were simply transferred straight from river boat to sea-going ship. An engraving by Newman and Co. of London preserved in the NLW shows a flimsy timber pier perhaps for ferry passengers but it may be significant that this also shows the never-built Dyfi viaduct and that it uses a woodblock very similar to another one in the NLW.

A small wharf or jetty may have existed briefly in the period immediately before the arrival of the railway; an axle and double-flanged wheel set for a 2' gauge railway were identified near the Medical Hall (SH 6140 9597) which ran from a quarry to the beach, and may possibly have provided ballast for ships (Green 1996 82).

It is not clear exactly what shape and form early railway wharf arrangements took. Some infrastructure would have been necessary to unload the construction locomotive and rolling stock from the barges and in 1869 the slate quarries were complaining about inadequate facilities (Green 1996, 83).

The building of the wharf and jetty was carried out in 1885 by Abraham Williams, described as a local contractor (but of whom nothing else is known – Boyd 1988, 319). An undated plan 'Aberdovey Harbour Plan of Wharves' reproduced in Boyd, shows a 'wharf' and a 'new wharf' as well as a 'pier', and shows facilities for cattle, warehouses and an arrangement of track which maximised the amount of locomotive operation possible in a confined space, though a horse is said to have been used on the tracks on the water's edge (Boyd 1988, 310, 318, 319). Green, however, offers the most detailed analysis of developments here. By 1887 the area to the east of the wharf included a coal yard, and cattle pens and a cattle dock connected to the landing stages. The wharf itself included two contiguous warehouses, the 'general warehouse' and 'Tomlinson's warehouse', a small store-shed, a coal bunker and a shed for the rail-travelling crane, a stable, an office, a custom house and harbourmaster's office, urinals and an 'elevator' – an overhead gantry arrangement apparently connected to Tomlinson's warehouse. Wagon turntables, both 12' and 15' diameter, were used, together with traversers. The harbour was rebuilt in 1969-70 and officially re-opened as a leisure facility on 28 September 1972 (Green 1996, 87-112). Photographs taken in the 1880s and subsequently indicate that the warehouses were built out of corrugated iron and

show bull-head-rail on chairs screwed down to longitudinal baulks; the crane was a vertical boiler affair, apparently not self-propelling, running on standard gauge rails (eg NLW: John Thomas 1544 B).

## **2.5 Industry**

### *2.5.1 Copper*

Copper was mined in the immediate hinterland of Aberdyfi. The name ‘Copperhill Street’ commemorates this activity. Fenton describes being shown Corbet’s ‘Copper Works’, which clearly included a smelter, then at a standstill until trials with coal could be made – implying that it was an experimental and short-lived venture. Much copper ore lay on the bank at the time of Fenton’s visit, and the whole enterprise was situated on ‘the broken ridge of Hills above the Port and just at the back of the Houses (Lloyd 1996, 27). This suggests two separate locations, perhaps one making use of the ores on the southern slopes of Cam Maethlon and the other in the region of SN 613 962.

### *2.5.2 Lead and silver*

Mining for lead and for silver and processing the ore was concentrated on the southern (Cardiganshire) side of the Dovey, but it seems likely that ores were boated over to Aberdyfi for transshipment on to sea-going-ships (Williams 1994, 199). There is a strong possibility of Roman smelting within Cors Fochno, and the discovery of a Prehistoric timber trackway on the southern part of the Cors by Cambria Archaeology in 2004 emphasised the continuity of human occupation and exploitation of the area from the earliest times. Within the Industrial and Modern period on the Cardiganshire side, Morris records a smelting-house and a silver mill at Garreg, and other smelting houses was also established at Ynys Hir and at Tal y Bont, though no smelting was carried out in the study area after the 18<sup>th</sup> century (Lewis 1996, 176). The Dyfi blast-furnace in Cardiganshire (SN 685 951), now conserved and interpreted by Cadw, was built by the Kendall family, dissenting entrepreneurs who also ran furnaces in Aberconwy, Shropshire, Yorkshire, Furness and Cheshire before making the transition from charcoal to coke production by building the Beaufort Furnace in Ebbw Vale in 1779 (Dinn 1988).

### *2.5.3 Slate*

Slate became by the 19<sup>th</sup> century the main export from Aberdyfi, and was quarried in several locations in the immediate hinterland. The Bryneglwys quarry began to produce for the market under the tenancy of John Pughe (1844-c. 1857), and is known to have used pack animals to export the slate over the ridge to a wharf at Pennal, where it was loaded into river craft and taken to Aberdyfi. It is possible that towards the end of this period, slate was being carted directly to Aberdyfi through Abergynolwyn down to the Dysinni valley; certainly, tradition spoke of horses being changed at Ty Mawr, just east of Tywyn, for the last leg of the journey. From 1866, by which time the McConnel brothers of Manchester were in charge, the Talyllyn Railway was taking slate as far as the standard-gauge at Tywyn wharf, when it was transhipped, either going directly to the English midlands by rail or making a short-haul journey to Aberdyfi for shipment (Boyd 1988). The Talyllyn Railway is of interest in the present context as the first of the Welsh slate quarry railways to give access to a standard gauge line rather than to tidal water; the nearby Cwm Ebol slab quarry built a short rail system to Pennal in 1865, which was the last to serve tidal water (Richards 2001, 73, 96).

Slate was quarried in the Dulas valley and its tributaries and taken by pack animals to Derwenlas to be loaded into boats of less than 50 tons which travelled down the tide to Aberdyfi to be loaded on to ocean-going vessels there. On 30 April 1859 the rail connection from Derwenlas to the Dulas valley quarries became operational, serving Cei Ellis, Cei Tafarn Isa and Cei Ward/Morben Wharf. The first two of these became disused with the construction of the standard-gauge railway between Machynlleth and Aberystwyth in January to July 1863, though Cei Ward seems to have struggled on for a few years. By 1866 an interchange yard had been installed at Machynlleth station and the lower section of the line became disused (Coulls 2006). However, this is unlikely to have diminished the amount of slate going on through Aberdyfi, such just as with the Bryneglwys quarry, it is likely that most of the slate transferred onto the standard gauge at

Machynlleth was short-haul traffic for shipment at Aberdyfi. The last slate cargo did not leave Aberdyfi until 1939 (Boyd 1988).

A small quarry operated at Trefri from 1864-84 and again in the 1880s. It may be one of the 'quarries of slate and stone' mentioned in the will of William Vaughan in 1677 (NLW Peniarth DA383). The deep slate pit is connected by tunnel to a chapel-like mill with long arched windows.

#### *2.5.4 Timber*

The export of timber was a staple of the local economy into the twentieth century, and was the single most important component of the local ship- and boat-building industry (qv). Morgan describes the timber arriving at the ship-building yards by road wagon (Morgan 1948, 116) but it is possible that in earlier times it may have been rafted along the Dyfi. There is, however, no evidence of dedicated timber yards at Aberdyfi, such as were to be found at Porthmadog, Caernarfon and Amlwch (Gwyn 2006), and the only timber-processing sites within the area are considered to be the saw-pits in the ship-yards.

#### *2.5.5 Flour milling*

A steam-powered multi-story mill was erected at Aberdyfi in 1881 and operated by a Mr Tomlin, who had a warehouse on the wharf from 1885 (Green 1996, 80). The mill site has been demolished and a modern housing estate built on the site.

#### *2.5.6 Ship- and boat-building*

Vessels for river and sea-traffic were fabricated at several points along the Dyfi, most notably at Derwenlas but this industry seems to have been concentrated at Aberdyfi. Three yards operated at Penhelyg and one in the vicinity of the present railway station. This industry lasted until about 1880 (Morgan 1948, 115) and its products were all in the Cardigan Bay tradition of the small coasting vessel – built out of organic material, and reflecting an increasingly common European and North American tradition. Only one locally-built vessel, so far as is known, had a steam engine, and that was installed by DeWinton's at Caernarfon.

Typically, the archaeology of the small ship-building creek is fragile, and at Aberdyfi the building of the railway and the creation of tourist infrastructure in the period from the 1860s onwards has effaced the surface archaeology of these sites.

### **2.6 Leisure and services**

Aberdyfi seems to have been developed as a leisure resort from the early 19th century – in other words, from before the classic 'industrial' period of its development, though leisure facilities were able to be expanded when industrial and transport needs contracted in the 1960s.

The origins of Aberdyfi's bathing facilities are unclear. A bathing machine proprietor is noted in the Directory for 1840, as is the proprietor of a Bath House. This may be the building known as the bath house; though the spur of land on which it is built is still undeveloped in an engraving of 1836, a building is shown here on an engraving of 1860 and the literary institute came to be housed in an immediately adjacent (and probably existing) building in 1897 (Birch 1982). The golf course was inaugurated in 1892, after golf had been played for some years (Darlington). A car-park was set out on the 'sand sidings' site in 1971 and the quay itself saw extensive change to fit it for its new purpose as a leisure focus (yachting and tourist information) in the same period.

The first life-boat station was built in 1837; stone-built, it measured 33' long by 14' 6" wide and remained in use until 1886 when it was replaced by a larger structure, which remained in use until 1931 (Morris, nd).

### 3. THE DEVELOPMENT OF ABERDYFI

The first settlement was located close to the coast edge, but on an approximate north-south alignment occupying a narrow valley that led into the hills beyond past the copper works. No medieval buildings are known to survive, however a report carried out by order of the Privy Council in 1565 identifies Aberdyfi as a settlement of only three houses (Lloyd 44). Associated with the ferry was Ty Mawr, later known as the (Royal) Raven Inn (PRN 25079) and now Royal House, which contains a beam dated 1645, and may have replaced an older and ruder ferry-house (Lloyd 1996, 69). Other hotels occupied the front, including the Ship Inn (now the Dovey Hotel), the construction of which is said to have been by Athelstan Owen in 1729 (Morgan 1948, 90). The Britannia Inn is not noted in the trade directories until 1856 but appears to be an older structure; the fact that it is built gable end-on to the turnpike suggests that it may have been a warehouse.

Lewis Morris' manuscript map of 1741-5 shows houses at Aberdyfi but appears to be following a convention whereby settlements are depicted as rows of dwellings on the same horizontal plane (LIRO WM/1905/23). It is clear however, that the Ynysymaengwyn estate was actively sponsoring house-building after Athelstan's death in 1731 in that one of the houses on Copperhill Street (at SH 6138 9603) bears a plaque reading 'Built By/Anne Owen/Widow/AD/1733' (Gwyn 2006, 193) – probably Athelstan's widow, the formidable Madam Owen, who died in 1741 (or 1760 according to another account), a keen builder who constructed the central block at Ynysymaengwyn and dedicated Tywyn almshouses (Lloyd 1996, 25). The style of the row of which this building forms part offers few local parallels, and may have more in common with the architecture of Cardiganshire.

Similar problems of dating affect some of the other buildings in Aberdyfi for instance at the corner of the quay and Copperhill Street, where the style is distinctively Georgian, but insufficient is known about the dates at which buildings of this sort were introduced into the area to offer any definite dates. What they do confirm is that by the early years of the 19<sup>th</sup> century, Aberdyfi was still probably a small and confined settlement but that it had seen several generations of growth under the patronage of the local landowner. The evolving pattern of the streetscape may reflect this also. What is effectively a Y-plan, with a road bifurcating to join a quayside road at two points, is a simple enough arrangement but allows for goods to be unloaded and loaded and where necessary displayed and bartered, and for carts and pack animals to circulate without clogging up the roads. The town would appear to have developed around this triangle, and during the 19<sup>th</sup> century spread to the east and west along the new roads developed from 1808 onwards.

To the east the building of a church in 1837 reflected the fact that Aberdyfi did not form a parish in its own right and that provision for visitors was necessary if the resort was to develop. St Peter's is set (appropriately enough) on a rock terrace, 'a neat Georgian Gothic box' with a chancel of 1890 by Henry Kennedy (Orbach, notes). Its axis runs parallel to the Penson road of 1827, reflecting not only the east-west alignment of Anglican churches but also the constricted space available to the estate. The Literary Institute was established in 1882 within an earlier bathing house, and the lifeboat house was built further east again in 1886. Steps lead up to the houses behind the sea front, as at Mervinia Terrace, and again by the Britannia Inn.

The building of the railway in the 1860s created a need for a second-generation of accommodation, buildings designed to meet the needs not of travellers but of holidaymakers. Some were clearly built in the period of land-speculation and development that followed the piecemeal sale of the Ynysymaengwyn estate in 1879. Others, however, are earlier and represent the earliest stages of the development of a tourist industry within Gwynedd. The trade directories indicate the stages of development. Pigot's *Directory* for 1828 (Pigot, 1174) – 'increasing in note as a bathing place', with a 'ladies' and gentlemen's academy' at Penhelig – a suggestion that not only was Aberdyfi trying to attract the seasonal trade but also attempting to appeal to *rentiers* who might settle in the area while their children were educated, as at Cheltenham. The Corbet Arms Hotel is noted as a 'posting place' (Pigot, 1828, 1174). Later directories speak of its suitability for invalids and make comparison with Torquay (1856, 1868).

It was during the second half of the 19<sup>th</sup> century that Glan-Dovey terrace and Bodfor Terrace were built along the front to the west of the early centre, and around a bend in the coast edge. These are typically tall Victorian terraces of three storeys with attics, designed to provide maximum accommodation with minimum ground plan. Bay windows typically reach from floor to eaves, with roofed dormers above.

#### **4. SUMMARY AND CONCLUSIONS OF THE ARCHAEOLOGICAL EVIDENCE**

Aberdyfi harbour forms part of a broader commercial/industrial land- and seascape which includes small harbours and quays elsewhere on the Dyfi estuary, the mines, quarries and smelters which fed them, the overland transport links which linked them together and the market town of Machynlleth. It effectively formed the entrepot for the southern part of Gwynedd and much of the northern part of Ceredigion.

Aberdyfi exemplifies the shift from a pre-Modern harbour based on a river-crossing and coastal trading to/from a rudimentary road system, with minimal quay facilities to rail/sea movement with small-scale but engineered facilities.

It also exemplifies that littoral commercial activities take a variety of forms; not only the transfer of goods from one mode of transport to another but also the development of leisure facilities – in this case, hotels, churches and chapels for no-local denominations, a literary institute, bathing and boating facilities. In the case of Aberdyfi, these have grown from the early 19<sup>th</sup> century, if not earlier, and have come to dominate the economy of the town.

#### **5. MANAGEMENT**

##### **5.1 Building management**

The central area of Aberdyfi forms a Conservation Area. Many of the older structures within this area are listed, though some which are clearly of considerable historic importance (eg the ‘Anne Owen’ terrace) are not.

##### **5.2 Harbour and littoral management**

The area includes part of the Cors Fochno and Dyfi *RAMSAR*.

Much of the area forms part of the Dyfi *SSSI*.

The area within the intertidal zone forms part of the *Snowdonia National Park*.

The study area includes part of the Dyfi *SPA*.

The study area includes part of the Dyfi *NNR*.

The study area includes part of the Dyfi *Biosphere Reserve*.

##### **5.3 Archaeological potential**

There is some potential for the survival of buried features along the length of coast identified as the present study area. These may be connected with mineral exploitation and processing, pre-Modern transport routes, and ship-building. The majority of standing buildings date from the 19<sup>th</sup> century or later but it is known that some include late medieval work, and detailed historic building survey has the potential to add to understanding of the archaeology of the area. There is also considerable potential for buried structures and artefactual recovery. This would have the particular merit of informing understanding of trade, commerce and transport within Gwynedd in the pre-Modern/Industrial period, an aspect of regional archaeology that is currently very little understood.



There is also considerable archaeological potential for further study that considers the archaeology of small ports and harbours within the context of the Dyfi estuary, rather than on a county-by-county basis. It is clear that Aberdyfi itself formed part of a littoral that was economically linked, and that this included smaller harbours, wharves and creeks such as Pennal, Derwenlas, Aberlerri/Borth and others. The archaeology of these sites is notoriously hard to identify, but Aberdyfi did not operate in isolation.

This in turn confirms the potential of the archaeology of the overland transport routes that served these locations – turnpike and pre-turnpike roads, and the railway systems.

#### **5.4 Recommendations**

It is recommended that a detailed historic building survey be carried out of the older structures within Aberdyfi, possibly through the medium of a Townscape Heritage Initiative.

An appropriate programme of archaeological works should be implemented prior to any below-ground intervention within the older area of Aberdyfi, and in advance of any major littoral/estuarine development.

## 6. PRINCIPAL SOURCES

### 6.1 Archival sources

#### *DRO*

Z/DAB (Ynysymaengwyn papers)

Z/M/620/1 (copy of Thomas Owen Morgan, *The Aberdovey Guide and Handbook*, second ed., 1863)

Z/M/806 (Cambrian Railways account book for port, 1882-8)

M Mackenzie, *The North Part of Cardigan Bay in North Wales* (n.d. – late 18<sup>th</sup> century? – shows smelting house on Dyfi)

#### *NLW*

Bob Owen papers 25/5,

Minor Deposits 289/B, ms 19266E, 21069ED

Peniarth DA383 (will of William Vaughan, 1677)

Western Sea Fisheries papers (Aberdyfi Mussel Fishers)

Gogerddan Estate box 75 (Gors Fochno enclosure).

#### *Llangefni Record Office*

WM/1905/23 (copy of Lewis Morris map, 1740s; original in hydrographic office)

#### *National archive, Kew*

State papers, Domestic, Elizabeth I, vol. 38, no. 30.

### 6.2 Visual material

#### *NLW*

Lithograph (view of Aberdyfi from south identical to following but shows steam boat and does not show jetty or bridge)

Lithograph 13558182 (view of Aberdyfi from south, with prayer by Rev. George Eyre Evans; shows railway bridge, timber pier)

Lithograph 13558182 (view by Newman and Co., 1860, from west)

Engraving 13558182 (view by A.F. Goddard from west 'Aberdovey North Wales', dated to 1880 by cataloguer but does not show railway)

Lithograph 13558182 (identified as 'Carnarvon: Wm Potter', dated c. 1835)

Lithograph 13558182 (allegedly 1880)

John Thomas jtm 025 (view of wharf and pier from east)

John Thomas jtm 025 (view of Penhelyg)

John Thomas jtm 013 (view of wharf and town from pier)

John Thomas jtm 017 (view of pier)

John Thomas jtm 019 (view from pier)

John Thomas jtm 025 (view of old lifeboat station)

John Thomas view of village pump

### 6.3 Secondary sources

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- Green CC, *Coast Lines of the Cambrian Railways* 2 (Wild Swan, 1996)
- Gwyn, D (2006), *Gwynedd: Inheriting a Revolution: The Archaeology of Industrialisation in North-west Wales* (Chichester, Phillimore)
- Lewis HM (no date): *A Riverside Story: The History of the Aberdyfi Wharf and jetty* (privately published)
- Lewis S (1838), *Topographical Dictionary* (London)
- Lewis WJ (1994), 'Lead Mining in Cardiganshire' in *Cardiganshire County History* 3 (Cardiganshire Antiquarian Society in association with the Royal Commission on the Ancient and Historical Monuments of Wales by the University of Wales Press)
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- Skempton AW (2002), *Biographical Dictionary of Civil Engineers Volume 1 1500 to 1830* (Institute of Civil Engineers)

#### **6.4 Unpublished notes**

Orbach J, notes towards Pevsner *Gwynedd* volume

#### **6.5 Websites**

Dafydd Wyn Wiliam, 'Morris, Lewis (1701–1765)', *Oxford Dictionary of National Biography*, Oxford University Press, 2004 [<http://www.oxforddnb.com/view/article/19313>, accessed 12 Jan 2007]

#### **6.6 Consultation**

Paul Edwards (harbourmaster)  
 David Williams (local historian)  
 Judith Alfrey (Cadw)  
 Julian Orbach

## APPENDIX I: GAZETTEER OF SITES

The gazetteer is divided into the following three sections: Group A are extant sites that are outstanding, often remaining as buildings in use. Group B are sites with good potential for the recovery of archaeology. Group C are sites with little or no potential for the recovery of archaeology. Within the gazetteer the sites are identified by their primary reference number (PRN) as used within the Gwynedd Historic Environment Record and are assessed according to their importance, that is national (A), regional (B), Local (C), Other (D) and unknown (E). The site names are printed in capitals if they are already part of the Historic Environment Record, and in lower case they have been identified during the course of the project. If it is a Listed Building the rating (as in GII, GII\* or GI) is given in 'Site Status Reference'.

### GROUP A: EXTANT SITES

#### **7289 ABERDYFI HARBOUR**

Assessment of Importance: A

Site Status Reference:

A nineteenth-century harbour development with a resort town immediately behind. The quay was re-developed in the 1960's.

Easting: 261360

Northing: 295830

#### **12468 SEA VIEW TERRACE, BRYN AWEL**

Assessment of Importance: B

Site Status Reference:

See PRN 25111

Easting: 261610

Northing: 295982

#### **12469 SEA VIEW TERRACE MIDLAND BANK;SHIP INN**

Assessment of Importance: B

Site Status Reference:

See PRN 25081

Easting: 261660

Northing: 295977

#### **12470 SEA VIEW TERRACE 19 AND 20**

Assessment of Importance: B

Site Status Reference: GII

Early C19 reflected pair of houses. Stucco with stucco quoins, slate roof with bracketed eaves, rubble end chimneys. Three storeys (over basement) plus hipped dormer to L house; 4 windows. Outer bays have 3-storey bow windows, tripartite sash glazing with central sash flanked by narrower sashes, horned sash glazing. To centre, pair of sash windows in moulded surrounds, 9-pane to second floor, 12-pane to first floor. Ground floor has, to centre, steps up to recessed porches, panelled to sides, shallow bracketed cornice supported by Doric columns with spiral reeding; panelled doors (some inserted glazing) rectangular overlight with webbed iron glazing. Iron railings to steps and basement area, those to area have alternating serpentine and straight uprights.

Easting: 261595

Northing: 296019

#### **12471 SEA VIEW TER.PIER+GREENS CAFES**

Assessment of Importance: B

Site Status Reference: GII

Early 19th century roughcast, 3 storey, plastered bow windows, wood hooded doorcase with fluted doric

columns steps and railings.

Easting: 261655

Northing: 295982

#### **25067 Railway**

Assessment of Importance: B

Site Status Reference:

The course of the railway system through Aberdyfi, from Dyfi Junction to Pwllheli, in operation, a single track railway typical of secondary practice of the 1860s.

Easting: 260696

Northing: 296028

**25068 Aberdyfi Station**

Assessment of Importance: B

Site Status Reference:

A station of the 1860s, downgraded in the late 20th century. There are now no goods facilities, no semaphore signalling and no passing loop. The south side (down) platform is disused and the wooden shelter has been removed; the station building of 1872 (on the north or 'up' side) has been converted into dwellings. Passenger shelter is now offered only a by a rudimentary shelter on the up platform. The station building is typical of mid-19th century practice, built of red brick with sandstone windows with a (very steeply-pitched) slate roof. The canopy was removed for re-erection at Llanuwchllyn on the Bala Lake Railway. Access road from main road.

Easting: 260688

Northing: 296037

**25069 Railway Bridge, Aberdyfi**

Assessment of Importance: B

Site Status Reference:

A steel bridge on brick abutments over the turnpike road.

Easting: 261124

Northing: 295988

**25071 Railway bridge, Aberdyfi**

Assessment of Importance: B

Site Status Reference:

A steel bridge on brick abutments over Copperhill Street.

Easting: 261342

Northing: 296070

**25072 Railway tunnel, Aberdyfi**

Assessment of Importance: B

Site Status Reference:

A single-track tunnel cut in 1862, extending from SH 6149 9607 to 6195 9615

Easting: 261504

Northing: 296074

**25073 Railway bridge, Aberdyfi**

Assessment of Importance: B

Site Status Reference:

A steel bridge on brick abutments over the A493 road immediately west of Penhelig Halt, a replacement of uncertain date for the original bridge put in the 1860s.

Easting: 262044

Northing: 296150

**25074 Penhelig Halt, Aberdyfi**

Assessment of Importance: B

Site Status Reference:

A typical example of a GWR halt of the inter-war period (it was opened in 1933); a single timber platform on the north side with a wooden 'pagoda' shelter, in good condition and painted white and green with red detailing. Access path from main road.

Easting: 262092

Northing: 296150

**25075 Railway bridge, Aberdyfi**

Assessment of Importance: B

Site Status Reference:

A steel bridge on brick abutments over the A493 road immediately east of Penhelig Halt.

Easting: 262134

Northing: 296144

**25076 Railway tunnel**

Assessment of Importance: B

Site Status Reference:

A single-track tunnel extending from 6219 9614 to 6236 9614; the western portal is built up of Ruabon brick.

Easting: 262193 Northing: 296142

**25077 Golf Course, Aberdyfi**

Assessment of Importance: C

Site Status Reference:

A golf-course, set out in 1892 (though there may have been earlier golf on the site).

Easting: 259510

Northing: 296652

**25079 Royal Raven Inn, Aberdyfi**

Assessment of Importance: B

Site Status Reference:

A building with a 19th century stucco façade but which contains a beam dated 1645 within. Situated on the corner of the turnpike, and Copperhill Street, and south-facing; the ground floor is now a café. The structure is contiguous with the butcher's shop noted below.

Easting: 261442

Northing: 295977

**25080 12 Prospect Place, Aberdyfi**

Assessment of Importance: B

Site Status Reference:

This butcher's shop on Copperhill Street has a mid-19th century ground-floor shop front (and is identified as 'Established 1861'), but the proportions of the structure in which it is housed are Georgian and appear to be contiguous with the Royal Raven Inn.

Easting: 261442

Northing: 295990

**25081 Dovey Inn, Aberdyfi**

Assessment of Importance: B

Site Status Reference:

A hotel occupying a central position on the A493 and in between its junctions with Copperhill Street and New Street. The façade has late 19th timbered bays and dormers, and a date-stone of 1729. It incorporates the structure of the Ship Inn of 1729, and was evidently rebuilt in the early and late 19th century.

Easting: 261469

Northing: 295994

**25082 Britannia Inn, Aberdyfi**

Assessment of Importance: B

Site Status Reference:

A public house with many 19th and 20th century accretions but in essence an earlier building, two storey, orientated north-south, possibly an inn of 18th century date or earlier, or possibly built as a store-house.

Easting: 261501

Northing: 296014

**25083 Old Custom House, Aberdyfi**

Assessment of Importance: B

Site Status Reference:

Now a pair of three-storey houses with a large central stone chimney, orientated east-west along the main road. The more westerly of the two bears the name 'The Old Custom House', and may be where Lewis Morris worked 1751-6. It is of substantial size and indicates the volume of trade passing through the harbour in the 18th century, as well perhaps as the resources of the Ynysymaengwn estate.

Easting: 261513

Northing: 296017

**25084 Literary Institute/Bath House, Aberdyfi**

Assessment of Importance: B

Site Status Reference:

A low stucco building with a timber veranda of 1897 on its north side, separated from the main road by a tiny courtyard. This has been the home of the Aberdovey Literary Institute (founded in 1882) since 1893, but the building is earlier, as appears to be shown on an engraving of 1860, when it formed one of a short row orientated north-south, though not one of 1836. One these functioned as the sea-baths.

Easting: 261707

Northing: 295997

**25085 Wharf, Aberdyfi**

Assessment of Importance: B

Site Status Reference:

A stone-built wharf enclosing the spur of rock on which the Literary Institute and the bath house are situated. It is marked (and a cross-section provided) on DRO: Z/CD/102 sheet 4 of 1865 but appears to post-date 1835.

Easting: 261713

Northing: 295988

**25086 Mussel Bath, Aberdyfi**

Assessment of Importance: C

Site Status Reference:

A rectangular-plan concrete mussel bath of post-war construction, on concrete supports.

Easting: 261912

Northing: 296065

**25091 Memorial Gardens, Aberdyfi**

Assessment of Importance: B

Site Status Reference:

A civic memorial garden laid out on a built-up quay projecting into the estuary. The garden commemorates a commando unit of German and other non-British nationals stationed at Aberdyfi during the Second World War.

Easting: 262154

Northing: 296090

**25094 Wharf, Aberdyfi**

Assessment of Importance: B

Site Status Reference:

In essence a quay built for the Cambrian Railways in 1885 by Abraham Williams, though it is possible that it includes earlier work; photographs show it as a timber-pile structure. The present steel-clad wharf wall was built approximately 1m out of the 19th century wharf walls, which are believed to survive. The wharf included as well as the buildings noted below, a stable, an office, urinals, an elevator, a crane shed and coal facilities.

Easting: 261419

Northing: 295921

**25097 Harbour buildings, Aberdyfi**

Assessment of Importance: C

Site Status Reference:

The buildings which have been erected on the wharf since the end of industrial use – a TIC, harbour-master's office, life-boat station etc., all in 'contemporary' idiom.

Easting: 261395

Northing: 295915

**25106 14 Glandovey Terrace, Aberdyfi**

Assessment of Importance: B

Site Status Reference: GII

Early C19 reflected pair of houses; ground floors converted to shops late C19/early C20. Stucco, slate roof, end chimneys. Three storeys plus attic; broad hipped dormer to each house. On first and second floors, outer bays have 2-storey bow windows with central 6-pane sash flanked by narrow sash, hornless sash glazing; 3 elliptical panels below each window of L house. To centre, second floor has paired 9-pane hornless sash windows each with semi-elliptical moulded fan above and quatrefoil between; L window retains flanking colonnettes. Paired 12-pane hornless sash windows to first floor under semi-elliptical panel, L window retains flanking colonnettes. Ground floor has, to centre, steps up to recessed porch (Doric columns), paired entrance doors, 6-panelled with rectangular overlight.

Ends each have later (early C20?) shopfront with door set back between large shop windows. (Cadw Listed Building description)

Easting: 261359

Northing: 295960

**25107 15 Glandovey Terrace, Aberdyfi**

Assessment of Importance: B

Site Status Reference: GII

Early C19 reflected pair of houses; ground floors converted to shops late C19/early C20. Stucco, slate roof, end chimneys. Three storeys plus attic; broad hipped dormer to each house. On first and second floors, outer bays have 2-storey bow windows with central 6-pane sash flanked by narrow sash, hornless sash glazing; 3 elliptical panels below each window of L house. To centre, second floor has paired 9-pane hornless sash windows each with semi-elliptical moulded fan above and quatrefoil between; L window retains flanking colonnettes. Paired 12-pane hornless sash windows to first floor under semi-elliptical panel, L window retains flanking colonnettes. Ground floor has, to centre, steps up to recessed porch (Doric columns), paired entrance doors, 6-panelled with rectangular overlight.

Ends each have later (early C20?) shopfront with door set back between large shop windows. (Cadw Listed Building description)

Easting: 261365

Northing: 295958

**25108 18 Glandovey Terrace, Aberdyfi**

Assessment of Importance: B

Site Status Reference: GII

Mid C19 house and shop. Stucco, stucco quoins, slate roof with boarded eaves, rendered end chimneys. Three storeys; 2 window front offset to R. Second floor windows square horned sashes (4 panes) in shallow moulded surrounds. First floor windows taller horned sashes (4 panes) in shallow moulded surrounds. Ground floor has, broad splayed shopfront (later C19); central glazed entrance doorway with two windows to each side; entablature over with C19 lettering 'Medical Hall' to centre, and 'Thomas & Co' to each side.

Easting: 261391

Northing: 295949

### **25109 St Peter's Church, Aberdyfi**

Assessment of Importance: B

Site Status Reference: GII

1832, chancel added later C19. Exterior: Tower, nave, chancel in grey brown stone, dressings (especially to chancel in bathstone, slate roofs. West tower with stepped crenellations; stepped diagonal buttresses, lancets to bell stage, small trefoil windows to lower stage (small rose window to W), clock; S tower doorway. Four bays to nave, stepped buttresses, 2-light windows with wooden cusped tracery. Lower chancel with small S window, E window in Decorated Style. Vestries/organ chamber to N. Interior: Entrance via tower S doorway; lobby with steps to L. Aisleless nave; rendered walls, coved and ribbed plaster ceiling. Exposed stone arch to chancel (brick walls, painted over), broad stone arch to organ chamber (N), hammer beam roof to chancel.

Good glass (1872) to E window in memory of Maria Jane Pugh, of Craig-y-Don, Aberdyfi.

Rear churchyard wall has arched entrance with stepped castellations over.

Easting: 261553

Northing: 296025

### **25110 Tabernacle Chapel, Aberdyfi**

Assessment of Importance: B

Site Status Reference: GII

On the eastern side of the town high above the seafront and reached by a long flight of steps, the gates of which are linked to the flanking terraced properties. The chapel site is cut into the hillside with high revetment wall.

History: Built 1864 to replace an earlier chapel founded in 1828.

Exterior: Simple-Classical, gable-fronted chapel; stone-built with slate roof. Symmetrical, 4-window, scribed-rendered main elevation has pediment treatment and raised stone quoins. Round-headed windows with bracketed hoodmoulds, sills and small-pane glazing; full-height outer windows lighting gallery stairs and

two smaller windows to centre over the paired round-headed entrances retaining panelled doors and fanlights. The front has two inscribed tablets; one to gable reads "Tabernacl Adeiladwyd 1864" and the other lower down to centre reads "Tabernacl ir Methodistiadd Calфинаidd yn y Flwyddyn 1828". Attached parallel to the right is the enlarged single-storey chapel hall with conical ventilators to the roof and gabled porch projecting forward from the front of the chapel with round-headed side doorway. 4-window pebbledashed side elevations have round-headed windows, similar to those on the main front; cement-rendered rear. The slate steps up to the chapel are closed at the base with iron gates with spearhead finials and panelled gate-piers, all

manufactured by the Coalbrookdale Company, Shropshire. Iron handrails with octagonal newels.

Interior: Scribed-rendered with roses and painted borders to plaster ceiling. The raked gallery is curved at the

front end and is carried on simply detailed cast-iron columns. The 'set fawr' and panelled platform remain and the segmental arch behind into the organ chamber is flanked by fluted Corinthian pilasters; organ case remains but organ taken out. At the time of inspection (April 1993) all the seating had been removed and the chapel was disused.

Easting: 261620

Northing: 296027

### **25111 26 Seaview Terrace, Aberdyfi**

Assessment of Importance: B

Site Status Reference: GII

Approximately 50m E of entrance to Tabernacle Chapel. Early C19 house. Pebbledash, slate roof, rendered end chimneys. Three storeys and basement, three windows; 9-pane hornless sashes to second floor; 12-pane hornless sashes to first floor. Ground floor centre has steps up to panelled entrance door flanked by Doric pillars, broad segmental overlight with fan glazing. To each side of door, later C19 splayed bay window cantilevered over basement, horned sash glazing. Small pane glazing to basement windows. L



return

has 9-pane sash window to second floor, lean-to garage. Iron railings with arrow-head finials to basement area and steps.

Easting: 261657

Northing: 296011

**25112 1 Brynhyfryd Terrace, Aberdyfi**

Assessment of Importance: B

Site Status Reference: GII

Terrace of 4 mid C19 houses. Two storeys plus attic, one window, rendered, slate roofs, rectangular chimneys, rendered. Each house has small attic triangular gable with small-pane hornless sash window set across eaves. First floor 12-pane hornless sash window; similar window to ground floor. Doorways set to R; classicising doorcases to Nos 2 & 4 (modern), modern doors.

Easting: 261704

Northing: 296040

**25113 2 Brynhyfryd Terrace, Aberdyfi**

Assessment of Importance: B

Site Status Reference: GII

Terrace of 4 mid C19 houses. Two storeys plus attic, one window, rendered, slate roofs, rectangular chimneys, rendered. Each house has small attic triangular gable with small-pane hornless sash window set across eaves. First floor 12-pane hornless sash window; similar window to ground floor. Doorways set to R; classicising doorcases to Nos 2 & 4 (modern), modern doors.

Easting: 261699

Northing: 296037

**25114 3 Brynhyfryd Terrace, Aberdyfi**

Assessment of Importance: B

Site Status Reference: GII

Terrace of 4 mid C19 houses. Two storeys plus attic, one window, rendered, slate roofs, rectangular chimneys, rendered. Each house has small attic triangular gable with small-pane hornless sash window set across eaves. First floor 12-pane hornless sash window; similar window to ground floor. Doorways set to R; classicising doorcases to Nos 2 & 4 (modern), modern doors.

Easting: 261694

Northing: 296036

**25115 4 Brynhyfryd Terrace, Aberdyfi**

Assessment of Importance: B

Site Status Reference: GII

Terrace of 4 mid C19 houses. Two storeys plus attic, one window, rendered, slate roofs, rectangular chimneys, rendered. Each house has small attic triangular gable with small-pane hornless sash window set across eaves. First floor 12-pane hornless sash window; similar window to ground floor. Doorways set to R; classicising doorcases to Nos 2 & 4 (modern), modern doors.

Easting: 261688

Northing: 296037

**25116 1 Mervinia Terrace, Aberdyfi**

Assessment of Importance: B

Site Status Reference: GII

To NE of Cliffside, reached by steps from road. Mid C19 terrace of 4 houses. Two-storeys, one window, rendered, slate roof. Grouped as two mirrored pairs, doors together (but No 1 has, to L, 2-storey extension, set back). First floor windows set at eaves, 12-pane hornless sashes; similar windows to ground floors. Modern boarded doors.

Easting: 261835

Northing: 296096

**25117 2 Mervinia Terrace, Aberdyfi**

Assessment of Importance: B

Site Status Reference: GII

To NE of Cliffside, reached by steps from road. Mid C19 terrace of 4 houses. Two-storeys, one window, rendered, slate roof. Grouped as two mirrored pairs, doors together (but No 1 has, to L, 2-storey extension, set back). First floor windows set at eaves, 12-pane hornless sashes; similar windows to ground floors. Modern boarded doors.

Easting: 261631

Northing: 296031

**25117 2 Mervinia Terrace, Aberdyfi**

Assessment of Importance: B

Site Status Reference: GII

To NE of Cliffside, reached by steps from road. Mid C19 terrace of 4 houses. Two-storeys, one window, rendered, slate roof. Grouped as two mirrored pairs, doors together (but No 1 has, to L, 2-storey extension,

set back). First floor windows set at eaves, 12-pane hornless sashes; similar windows to ground floors. Modern boarded doors.

Easting: 261838

Northing: 296097

**25118 3 Mervinia Terrace, Aberdyfi**

Assessment of Importance: B

Site Status Reference: GII

To NE of Cliffside, reached by steps from road. Mid C19 terrace of 4 houses. Two-storeys, one window, rendered, slate roof. Grouped as two mirrored pairs, doors together (but No 1 has, to L, 2-storey extension, set back). First floor windows set at eaves, 12-pane hornless sashes; similar windows to ground floors. Modern boarded doors.

Easting: 261843

Northing: 296098

**25119 4, Mervinia Terrace, Aberdyfi**

Assessment of Importance: B

Site Status Reference: GII

To NE of Cliffside, reached by steps from road. Mid C19 terrace of 4 houses. Two-storeys, one window, rendered, slate roof. Grouped as two mirrored pairs, doors together (but No 1 has, to L, 2-storey extension, set back). First floor windows set at eaves, 12-pane hornless sashes; similar windows to ground floors. Modern boarded doors.

Easting: 261847

Northing: 296099

**25120 1 Penhelig Lodge, Aberdyfi**

Assessment of Importance: B

Site Status Reference: GII

History: Early C19 terrace of 3 houses, shown on early engravings of Aberdyfi, circa 1837. Originally three 3-window houses like central house, at one stage school, from later C19 altered to become lodge of Penhelig House (early C20 photos show upper storey with applied half-timbering). Two storeys, rubble, rendered ground floor with band course between floors, slate roof with 4 small rendered chimneys. L house has two windows with first floor small-pane sashes set at eaves; on ground floor, to L small-pane sash window, to R doorway. Central house has, on first floor, three 9-pane hornless sash windows; on ground floor, central doorway flanked by small-pane sash windows. R house has two windows with first floor small-pane sashes set at eaves; ground floor has 2 tripartite casement windows to front, small-pane sash to R end. To rear of L house, extension with gabled half-timbered oriel.

Easting: 262106

Northing: 296178

**25121 2 Penhelig Lodge, Aberdyfi**

Assessment of Importance: B

Site Status Reference: GII

History: Early C19 terrace of 3 houses, shown on early engravings of Aberdyfi, circa 1837. Originally three 3-window houses like central house, at one stage school, from later C19 altered to become lodge of Penhelig House (early C20 photos show upper storey with applied half-timbering). Two storeys, rubble, rendered ground floor with band course between floors, slate roof with 4 small rendered chimneys. L house has two windows with first floor small-pane sashes set at eaves; on ground floor, to L small-pane sash window, to R doorway. Central house has, on first floor, three 9-pane hornless sash windows; on ground floor, central doorway flanked by small-pane sash windows. R house has two windows with first floor small-pane sashes set at eaves; ground floor has 2 tripartite casement windows to front, small-pane sash to R end. To rear of L house, extension with gabled half-timbered oriel.

Easting: 262114

Northing: 296174

**25122 3 Penhelig Lodge, Aberdyfi**

Assessment of Importance: B

Site Status Reference: GII

History: Early C19 terrace of 3 houses, shown on early engravings of Aberdyfi, circa 1837. Originally three 3-window houses like central house, at one stage school, from later C19 altered to become lodge of Penhelig House (early C20 photos show upper storey with applied half-timbering). Two storeys, rubble, rendered ground floor with band course between floors, slate roof with 4 small rendered chimneys. L house has two windows with first floor small-pane sashes set at eaves; on ground floor, to L small-pane sash window, to R doorway. Central house has, on first floor, three 9-pane hornless sash windows; on ground floor, central doorway flanked by small-pane sash windows. R house has two windows with first floor small-pane sashes set at eaves; ground floor has 2 tripartite casement windows to front, small-pane sash to R end. To rear of L house, extension with gabled half-timbered oriel.

Easting: 262121

Northing: 296171

**25123 1 Evans Terrace, Aberdyfi**

Assessment of Importance: B

Site Status Reference: GII

To rear of 10-17 Copperhill street. Terrace of 6 houses one of which (No 4) bears datestone 'Built by Edward Price 1827'. Two storeys, rendered, slate roofs, rendered chimneys. Nos 1-3 are one-window houses. No 1 has sash windows with glazing bars removed, modern door. No 2 has modern glazing, modern door. No 3 has small-pane Yorkshire (horizontal) sashes, boarded door. Nos 4 & 5 are two-window (double fronted) houses; No 4 has sash windows with glazing bars removed, boarded door; No 5 has small-pane Yorkshire (horizontal) sashes, boarded door. No 6, one-window house, modern glazing and door.

Easting: 261379

Northing: 296031

**25124 2 Evans Terrace, Aberdyfi**

Assessment of Importance: B

Site Status Reference: GII

To rear of 10-17 Copperhill street. Terrace of 6 houses one of which (No 4) bears datestone 'Built by Edward Price 1827'. Two storeys, rendered, slate roofs, rendered chimneys. Nos 1-3 are one-window houses. No 1 has sash windows with glazing bars removed, modern door. No 2 has modern glazing, modern door. No 3 has small-pane Yorkshire (horizontal) sashes, boarded door. Nos 4 & 5 are two-window (double fronted) houses; No 4 has sash windows with glazing bars removed, boarded door; No 5 has small-pane Yorkshire (horizontal) sashes, boarded door. No 6, one-window house, modern glazing and door.

Easting: 261383

Northing: 296028

**25125 3 Evans Terrace, Aberdyfi**

Assessment of Importance: B

Site Status Reference: GII

To rear of 10-17 Copperhill street. Terrace of 6 houses one of which (No 4) bears datestone 'Built by Edward Price 1827'. Two storeys, rendered, slate roofs, rendered chimneys. Nos 1-3 are one-window houses. No 1 has sash windows with glazing bars removed, modern door. No 2 has modern glazing, modern door. No 3 has small-pane Yorkshire (horizontal) sashes, boarded door. Nos 4 & 5 are two-window (double fronted) houses; No 4 has sash windows with glazing bars removed, boarded door; No 5 has small-pane Yorkshire (horizontal) sashes, boarded door. No 6, one-window house, modern glazing and door.

Easting: 261386

Northing: 296026

**25126 4 Evans Terrace, Aberdyfi**

Assessment of Importance: B

Site Status Reference: GII

To rear of 10-17 Copperhill street. Terrace of 6 houses one of which (No 4) bears datestone 'Built by Edward Price 1827'. Two storeys, rendered, slate roofs, rendered chimneys. Nos 1-3 are one-window houses. No 1 has sash windows with glazing bars removed, modern door. No 2 has modern glazing, modern door. No 3 has small-pane Yorkshire (horizontal) sashes, boarded door. Nos 4 & 5 are two-window (double fronted) houses; No 4 has sash windows with glazing bars removed, boarded door; No 5 has small-pane Yorkshire (horizontal) sashes, boarded door. No 6, one-window house, modern glazing and door.

Easting: 261391

Northing: 296023

**25127 5 Evans Terrace, Aberdyfi**

Assessment of Importance: B

Site Status Reference: GII

To rear of 10-17 Copperhill street. Terrace of 6 houses one of which (No 4) bears datestone 'Built by Edward Price 1827'. Two storeys, rendered, slate roofs, rendered chimneys. Nos 1-3 are one-window houses. No 1 has sash windows with glazing bars removed, modern door. No 2 has modern glazing, modern door. No 3 has small-pane Yorkshire (horizontal) sashes, boarded door. Nos 4 & 5 are two-window (double fronted) houses; No 4 has sash windows with glazing bars removed, boarded door; No 5 has small-pane Yorkshire (horizontal) sashes, boarded door. No 6, one-window house, modern glazing and door.

Easting: 261395

Northing: 296019

**25128 6 Evans Terrace, Aberdyfi**

Assessment of Importance: B

Site Status Reference: GII

To rear of 10-17 Copperhill street. Terrace of 6 houses one of which (No 4) bears datestone 'Built by Edward Price 1827'. Two storeys, rendered, slate roofs, rendered chimneys. Nos 1-3 are one-window houses. No 1 has sash windows with glazing bars removed, modern door. No 2 has modern glazing, modern door. No 3 has small-pane Yorkshire (horizontal) sashes, boarded door. Nos 4 & 5 are two-window (double fronted) houses; No 4 has sash windows with glazing bars removed, boarded door; No 5 has small-pane Yorkshire (horizontal) sashes, boarded door. No 6, one-window house, modern glazing and door.

Easting: 261400

Northing: 296016

**25129 5 New Street, Aberdyfi**

Assessment of Importance: B

Site Status Reference: GII

Three storey, 2-window house; rendered, slate roof, brick chimneys. Second floor windows hornless sashes (once 9-pane, glazing bars removed from lower sashes); first floor windows horned sashes, glazing bars removed. On ground floor, doorway (to right) and 4-pane sash window (to left) both offset to right. To L, lean-to extension with door.

Easting: 261440

Northing: 296036

**25130 6 New Street, Aberdyfi**

Assessment of Importance: B

Site Status Reference: GII

To N of former hall in Chapel Square. Included as part of well-preserved group facing town square.

Easting: 261446

Northing: 296034

**25131 7 New Street, Aberdyfi**

Assessment of Importance: B

Site Status Reference: GII

To N of former hall in Chapel Square. Included as part of well-preserved group facing town square.

Easting: 261450

Northing: 296033

**25132 8 New Street, Aberdyfi**

Assessment of Importance: B

Site Status Reference: GII

To N of former hall in Chapel Square. Included as part of well-preserved group facing town square.

Easting: 261456

Northing: 296032

**25133 9 New Street, Aberdyfi**

Assessment of Importance: B

Site Status Reference: GII

To N of former hall in Chapel Square. Included as part of well-preserved group facing town square.

Easting: 261460

Northing: 296031

**25134 10 New Street, Aberdyfi**

Assessment of Importance: B

Site Status Reference: GII

To N of former hall in Chapel Square. Included as part of well-preserved group facing town square.

Easting: 261467

Northing: 296029

**25135 11 New Street, Aberdyfi**

Assessment of Importance: B

Site Status Reference: GII

To N of former hall in Chapel Square. Included as part of well-preserved group facing town square.

Easting: 261474

Northing: 296026

**25136 12 New Street, Aberdyfi**

Assessment of Importance: B

Site Status Reference: GII

To N of former hall in Chapel Square. Included as part of well-preserved group facing town square.

Easting: 261482

Northing: 296024

**25137 Walls and railings to St Peters Church, Aberdyfi**

Assessment of Importance: B

Site Status Reference: GII

Churchyard entered from S via Gothic-arched gateway (with wrought-iron gates), sundial above on shaped pediment, arch leads to ramped walkway with cast-iron posts, and rails; iron railings to churchyard. Included for group value with St Peter's Church.

Easting: 261554

Northing: 296015

**CATEGORY B: BURIED SITES WITH ARCHAEOLOGICAL POTENTIAL**

**16946 ANCHOR, ABERDYFI HARBOUR**

Assessment of Importance: E

Site Status Reference:

An old anchor which resided on the front of Aberdovey for more than 20 years. Local residents discovered by chance that the anchor was being used as a sinker for the replaced fairway buoy. The anchor is an admiralty pattern in use from the late 18th century, a similar anchor lies on the wharf in Dundee having come from HMS Brilliant which was used as a training ship in the mid-19th century.

It was brought to Aberdyfi about that time to be used as an anchor for the chain ferry that existed across the river before the railway tunnels were completed along the north side of the estuary. One fluke was cut off to avoid damaging the hull of the ferry. It was discovered by local people at a time when the water was low and the sand washed away. The mooring for the south end of the chain is still in situ consisting of a very large iron flywheel, bits of the chain were still on the river bed some years ago.

The anchor was unique in having a round oak stock, often the stock for this type of anchor was square. Through neglect the stock was badly rotted,

Easting: 261436

Northing: 295911

**20402 SLATE QUARRY, ALLTGOCH**

Assessment of Importance: C

Site Status Reference:

Easting: 262000

Northing: 296400

**21143 LEAD/COPPER MINE, CORBET DOVEY**

Assessment of Importance: C

Site Status Reference:

Easting: 261200

Northing: 296000

**21144 LEAD & COPPER MINE, BALKAN HILL]**

Assessment of Importance: C

Site Status Reference:

Easting: 261700

Northing: 296300

**21145 QUARRY, TREFEDDIAN**

Assessment of Importance: C

Site Status Reference:

Easting: 260200

Northing: 296700

**21149 QUARRY, YR HORON**

Assessment of Importance: C

Site Status Reference:

Easting: 260600

Northing: 296200

- 21150 QUARRY, CRAIG Y DON**  
 Assessment of Importance: C Site Status Reference:  
 Easting: 261100 Northing: 296000
- 21151 SHAFT, ALLT GOCH**  
 Assessment of Importance: C Site Status Reference:  
 Easting: 261900 Northing: 296700
- 21881 PENHELIG MINE**  
 Assessment of Importance: C Site Status Reference:  
 Easting: 262000 Northing: 296400
- 21939 ALLT GOCH MINE**  
 Assessment of Importance: C Site Status Reference:  
 Geology: Trials driven in rocks of Silurian age for lead ore. History: nothing is known of the history of this site.  
 Easting: 261700 Northing: 296300
- 25066 Gasworks, Aberdyfi**  
 Assessment of Importance: C Site Status Reference:  
 The site of the gasworks. No above-ground remains are evident.  
 Easting: 259748 Northing: 296601
- 25070 Railway sidings, Aberdyfi**  
 Assessment of Importance: C Site Status Reference:  
 The site of the 'sand sidings' from the station to the quay; the area became a car-park in 1971  
 Easting: 261245 Northing: 295946
- 25078 Stone Quarries, Aberdyfi**  
 Assessment of Importance: B Site Status Reference:  
 A series of small quarries, some of which may have provided road-stone and some ballast for ships. An axle-set for a 2' gauge double-flanged wagon was discovered at approximately SH 6140 9597, suggesting a rail system extending to a quay.  
 Easting: 261124 Northing: 296010
- 25088 Piles, Aberdyfi**  
 Assessment of Importance: A Site Status Reference:  
 A row of timber piles and some vertical sections of rail set in the sand at this point, suggesting that a jetty existed here.  
 Easting: 262024 Northing: 296095
- 25095 Jetty, site of, Aberdyfi**  
 Assessment of Importance: D Site Status Reference:  
 The site of a timber pile jetty (the present wooden wharf is of late 20th century construction) which carried a set of standard gauge rails. No surviving remains are apparent above the water-line. Originally built in 1885. The present jetty was built in the 1960s after the 1885 structure was found to be affected by Teredo worm.  
 Easting: 261404 Northing: 295875
- 25098 Upper landing stage, Aberdyfi**  
 Assessment of Importance: D Site Status Reference:  
 A timber landing stage, possibly older than the main part of the wharf, which gave access to lairage facilities

at its north-western extremity. Now demolished.

Easting: 261383

Northing: 295881

**25100 Turnpike Road, Aberdyfi**

Assessment of Importance: B

Site Status Reference:

In essence the turnpike built by Thomas Penson in the 1820s but substantially improved in the 20th century for road traffic.

Easting: 259746

Northing: 296849

**25101 Hen Ffordd Corbet, Aberdyfi**

Assessment of Importance: C

Site Status Reference:

The early 19th century road built by the Ynysmaengwyn estate, allegedly so near the tidal level that it was frequently inundated. No traces are apparent within the immediate vicinity of the town, and it is possible that they have been largely effaced by the railway and the turnpike road but a trace is apparent to the east which coincides with the route of the map as shown in the turnpike survey of 1823.

Easting: 262351

Northing: 296126

**25102 Copperworks, Aberdyfi**

Assessment of Importance: E

Site Status Reference:

A copper smelter noted by Fenton and described as being 'just at the back of the Houses'. Its location is therefore a matter of guesswork and no maps show it or hint at its location, which has suggested here on the basis of the assumed extent of the settlement in the early 19th century. It is possible that the railway may have largely destroyed surface evidence.

Easting: 261339

Northing: 296083

**25105 Tannery, Dolgellau**

Assessment of Importance: E

Site Status Reference:

The site of a tannery marked on the 1900 OS map, with leat and water source from the river alongside.

Easting: 259871

Northing: 296579

**GROUP C: BURIED SITES WITH POOR ARCHAEOLOGICAL POTENTIAL**

**4954 TWO BRONZE COINS - GREEK**

Assessment of Importance: D

Site Status Reference:

Easting: 261000

Northing: 296000

**25065 Melin Ardudwy**

Assessment of Importance: D

Site Status Reference:

The site of a multi-storey steam flour roller mill of late 19th century date. No above-ground remains are evident and the site has become a housing estate.

Easting: 260183

Northing: 296159

**25087 Smithy, Aberdyfi**

Assessment of Importance: D

Site Status Reference:

A smithy is marked at this point up to the 1902 ordnance survey. It may have been associated with the shipyard immediately to the east.

Easting: 261979

Northing: 296093

**25089 Wharf, Aberdyfi**

Assessment of Importance: D

Site Status Reference:

The c. 1880 photograph in NLW (jta011) shows a built-up stone wharf at this point. It is not marked on DRO: Z/CD/102 sheet 4 of 1865. The sub-rectangular 'apron' plan of the wharf does not suggest that it was a very consciously engineered structure and it had no rail access. The photograph shows no activity taking place on it, and it is unlikely to have formed part of the ship-building process. The area immediately to the west was infilled between 1887 and 1902, and the Penhelyg Terrace has been built on its site. It may represent spoil from the railway tunnels.

Easting: 262091

Northing: 296105

**25090 Shipyard, Aberdyfi**

Assessment of Importance: D

Site Status Reference:

A possible shipyard at this location is indicated on a sloping beach on the John Thomas photograph of c. 1880 (NLW: jta011).

Easting: 262144

Northing: 296076

**25092 Former shipyard, Aberdyfi**

Assessment of Importance: D

Site Status Reference:

Morgan refers to a shipyard immediately to the west of Bryn Llestair/Picnic Island.

Easting: 262559

Northing: 296202

**25093 Former shipyard, Aberdyfi**

Assessment of Importance: D

Site Status Reference:

Morgan refers to a shipyard immediately to the east of Bryn Llestair/Picnic Island.

Easting: 262726

Northing: 296229

**25096 Buildings, site of, Aberdyfi**

Assessment of Importance: D

Site Status Reference:

The buildings which existed on the wharf in the period 1885 to 1972. This included two substantial two-storey corrugated iron bow-roofed sheds, built as part of the harbour developments carried out by the Cambrian Railways in 1885. One was the 'general warehouse and the other was 'Tomlin's warehouse', associated with the steam flour mill. Now demolished, and the site occupied by buildings of late 20th century construction. Other buildings included offices, a urinal, coal stores and a shed for the railed crane.

Easting: 261412

Northing: 295920

**25099 Lower landing stage**

Assessment of Importance: D

Site Status Reference:

A timber landing stage, possibly older than the main part of the wharf, which gave access to lairage facilities at its north-western extremity. Now demolished.

Easting: 261379

Northing: 295880

**25104 Site of shipyard, Aberdyfi**

Assessment of Importance: E

Site Status Reference:

No features are apparent of this site other than those mentioned above but a shipyard is suggested here by Morgan in Brief Glory, confirmed by a photograph of c. 1880 in the John Thomas collection at NLW (jta011). There has been realignment of the water's edge at this point, the effect of which has been to fill in the space between the wharf to the east and the turnpike to the east. John Thomas' photograph dates from the period when the yard would have been on its last legs or had already closed, and suggests little or no equipment and no buildings.

Easting: 262013

Northing: 296109



## APPENDIX II

*The draft description for Aberdyfi for the forthcoming Pevsner Gwynedd volume prepared by Julian Orbach reads as follows:*

Aberdyfi has the character of a seaside resort started with the coast road from Pennal in 1827, but has a much longer history. At the Council of Aberdyfi in 1216, Llywelyn the Great imposed his authority on the princes of Deheubarth and Powys. It was part of the Ynysmaengwyn (Bryncrug) estate from the C15. Herring-fishing and the shipping of copper and lead from the hills behind were staples until the early C19. The Customs House for the coast was here until 1763. Typically the early settlement did not face the sea, but ran inland up a narrow valley.

After 1827 a single row of lodging houses spread along the seafront, the plots cut back into the rock, a tourist hotel was built to the W in 1829, and the church was added in 1837. The railway arriving in 1863, first as just a coastal stretch running N, but from 1867 linked to the main line, aroused the same inflated expectations of tourism as at Aberystwyth. The Corbet Arms hotel was rebuilt by Thomas Savin, the railway promoter, but like his hotels at Borth and Aberystwyth, it was not a success and was for sale by 1869. The seafront lodging houses extended outward through the later C19 and villas were built on the slopes behind, though not in quantity before 1900. These villas are typically stuccoed in the 1880s moving to a seaside 'Old English' mix of roughcast, red brick and half-timber.

There was small-scale ship-building at Penhelig c. 1840-80, lead mining in Cwm Maethlon (Tywyn) to the N and a slate quarry to the E. The railway stimulated the port as an outlet for the export of slate, with a wharf being built in 1882.

ST PETER, Seaview Terrace. 1837. Set on a rock terrace, the church is a neat Georgian Gothic box with a W tower, of long blocks of slaty stone. Chancel of 1890, by *Henry Kennedy*, in sympathy. The nave has buttresses and acutely pointed windows, the timber tracery framing delicate cast-iron glazing, surely later. The tower has lancet bell-lights, and inside, curving slate stairs for a lost gallery. Plaster nave ceiling, Tudor-arched with thin ribs. The chancel is lined in painted brick under a fussy hammerbeam roof. In the nave, C19 painted BOARDS with texts in Welsh. Later C19 FONT, eight-lobed. Other FITTINGS of the 1920s, the rails with Masonic symbols, 1924. – STAINED GLASS. E window, 1873, by Ward & Hughes, busy scenes. Chancel S lancet, c. 1890, subdued colours with streaky turquoise. Nave S two-light, 1924, conventional. – MONUMENTS. Susan Scott + 1844, crocketed Gothic, by *Carline & Dodson* of Shrewsbury. W end, Baron Atkin of Aberdovey + 1944, a small brass, the figure in judge's robes, by *Peter Morton*.

Contemporary cast-iron terrace railings, and plain Gothic LYCHGATE below, with a sundial.

CHRIST THE KING RC, Tywyn Road. 1974? by *Weightman & Bullen*. Economical cube of block-work with low timber cross gables, two glazed, attached to a flat-roofed hall, with folding screen to the church. – Madonna statue by *John Skelton*.

CONGREGATIONAL CHAPEL, Seaview Terrace. 1880. Quite small, Gothic in green and grey stone, with a traceried window and a porch tower to r. under an octagonal spire.

ENGLISH PRESBYTERIAN CHURCH, Seaview Terrace. 1893, by *Richard Davies*. Small, stuccoed with a big arched stone window, and a squat porch tower to the r. capped by a slated pyramid. The detail is slightly Romanesque, the feeling American.

TABERNACL CALVINISTIC METHODIST CHAPEL (former), Seaview Terrace. 1863-4. Now flats. Splendidly set at the head of a flight of slate steps with good *Coalbrookdale* iron gates. Broad plain stuccoed front with quoins, arched windows, and paired doors, all with arched hoods.

BETHEL WELSH WESLEYAN CHAPEL, Chapel Square. 1868. A broad pedimental gable, like Tabernacl, but Gothic with plain lancets. Unusually the front was of brick, stuccoed over in the 1920s.

*Perambulation.*

The DOVEY HOTEL, on the seafront, although disguised with late C19 timbered bays and dormers is an earlier C19 rebuilding of the Ship Inn from which the 1729 date-stone comes. COPPER HILL STREET runs back to CHAPEL SQUARE, named not for the Wesleyan chapel q.v. but for the first Calvinistic Methodist chapel, 1828, by the *Rev. Richard Humphreys* of Dyffryn, now DOVEY MARINE. If the original chapel, it is substantial for the date. The terracotta arch and a gable plaque may be relics of its use as a Market Hall from 1864. In NEW STREET behind, a three-storey stuccoed row, earlier to mid C19. COPPER HILL STREET continues with mixed stuccoed houses, two-storey on the W, No. 14 dated 1733, altered, some three-storey on the E. Behind the W side, EVANS TERRACE, 1827, a pretty two-storey row, one house with horizontal-sliding sashes. Beyond the railway, TAI NEWYDDION, well-designed roughcast cottages of c. 1910-20, the upper block with eyebrow eaves.

Back to the seafront, ROYAL HOUSE on the W corner was the Raven Inn, altered C19 stucco now, but a beam dated 1645 inside. Opposite, the QUAY, the wharf of 1882 completely rebuilt in 1968-70. Long boathouse remodelled in 1995 with four sets of large double doors. After the Congregational Chapel q.v., GLANDOVEY TERRACE, stuccoed, three storeys and attic, what unity there was lost in alteration. The terrace bends at a late C19 pair, once the HARBOUR HOTEL, a tall four storeys with square bays on walls of green stone, more Barmouth than Aberdyfi. Nos. 14-15 is earlier C19 like those by the church (see below), three storeys with delicate two-storey curved bows. Good paired doorcases with overlights, columns and cornice. Then taller terraces with canted three-storey bays, finishing at BODFOR TERRACE, c. 1890-1900, two blocks, with red terracotta columns to the bays and arched doorways.

From the Dovey Hotel, SEAVIEW TERRACE runs E. The OLD CUSTOMS HOUSE, a spoilt three-storey pair with large central stone chimney may be where Lewis Morris was Customs Officer 1751-6. Next, three large paired lodging houses shown in a view of 1834. Nos 15-16 is altered, just one traceried overlight left, behind brown terracotta early C20 arches. Past the parish church q.v., Nos. 17-18 has a good Doric doorcase with triglyphs, but Nos. 19-20 is the best, with three-storey curved bows and doorcase with cornice on spiral-fluted columns. The detail echoes Aberystwyth: the curved bows on Pier Street, the spiral fluting on the Old Black Lion, Bridge Street. Past the gates to Tabernacl chapel q.v., Nos. 24-25, of c. 1900, much busier, with bays and curved-topped upper windows. No 26, BRYNAWEL, a single earlier C19 house, has a doorway with columns and elliptical fanlight. Opposite the English Presbyterian church q.v., the INSTITUTE, a reading room of 1882 in Bath House, a low stucco building built for sea-bathing in the earlier C19. Set back in a tiny courtyard with a festive timber veranda of 1897. To E, on TERRACE ROAD, Nos. 1-3 CLIFFSIDE, c. 1885, stuccoed terrace with Gothic columns to hefty bays, and, set back, MERVINIA TERRACE, mid C19 small row of four, then the LIFEBOAT HOUSE of 1887 with RNLBI plaque.

Beyond, at PENHELIG, past the railway bridge, NANTIESYN, a short row of eight three-storey houses and PENHELIG LODGE, row of stone cottages, both shown in a view of 1837. Running N, two drives, on the W to the gabled former VICARAGE, later C19, and on the E, to PLAS PENHELIG, a large Edwardian villa, 1902-5, roughcast with red tiled roofs, the front double-gabled with an octagonal corner bay.

W of the town, under the railway bridge, PRIMARY SCHOOL of 1968 to N, on the site of the Corbet Arms Hotel, and the RC church q.v. to the S. A lane runs S to the STATION, c. 1865, steep-roofed in red brick with sandstone windows. The hill above the school was built up with early C20 villas, several with the octagonal corner bay that seems a favourite motif. On the r. HENDRE, with tiny-paned windows and on the l. GLASLYN, 1914, quirky, with different gables, one with a chimney attached and undulating parapets.

OUTWARD BOUND CENTRE, 1 m. E. Established 1941 in the grounds of Bryneithin, a gaunt later C19 twin-gabled stone house. Stepped accommodation blocks on the slope, 1989-91, by *Mitchell & Holden* of Pembroke.

TREFRI HALL, 1 ¼ m. E. Delightfully situated earlier C19 picturesque villa on a headland. Stone, roughcast above, three bays, with bargeboarded centre gable and very long ground floor sash windows with hoodmoulds.

TREFRI FAWR, ¾ m N of Trefri Hall. Characterful early C18 farmhouse with three irregularly-spaced gabled stone dormers. To the r., a small barn dated 1738.

FRONGOCH SLATE QUARRY, 2 ½ m. E. Worked from 1864-84. The deep slate pit is connected by tunnel to a chapel-like engine-house with long arched windows.

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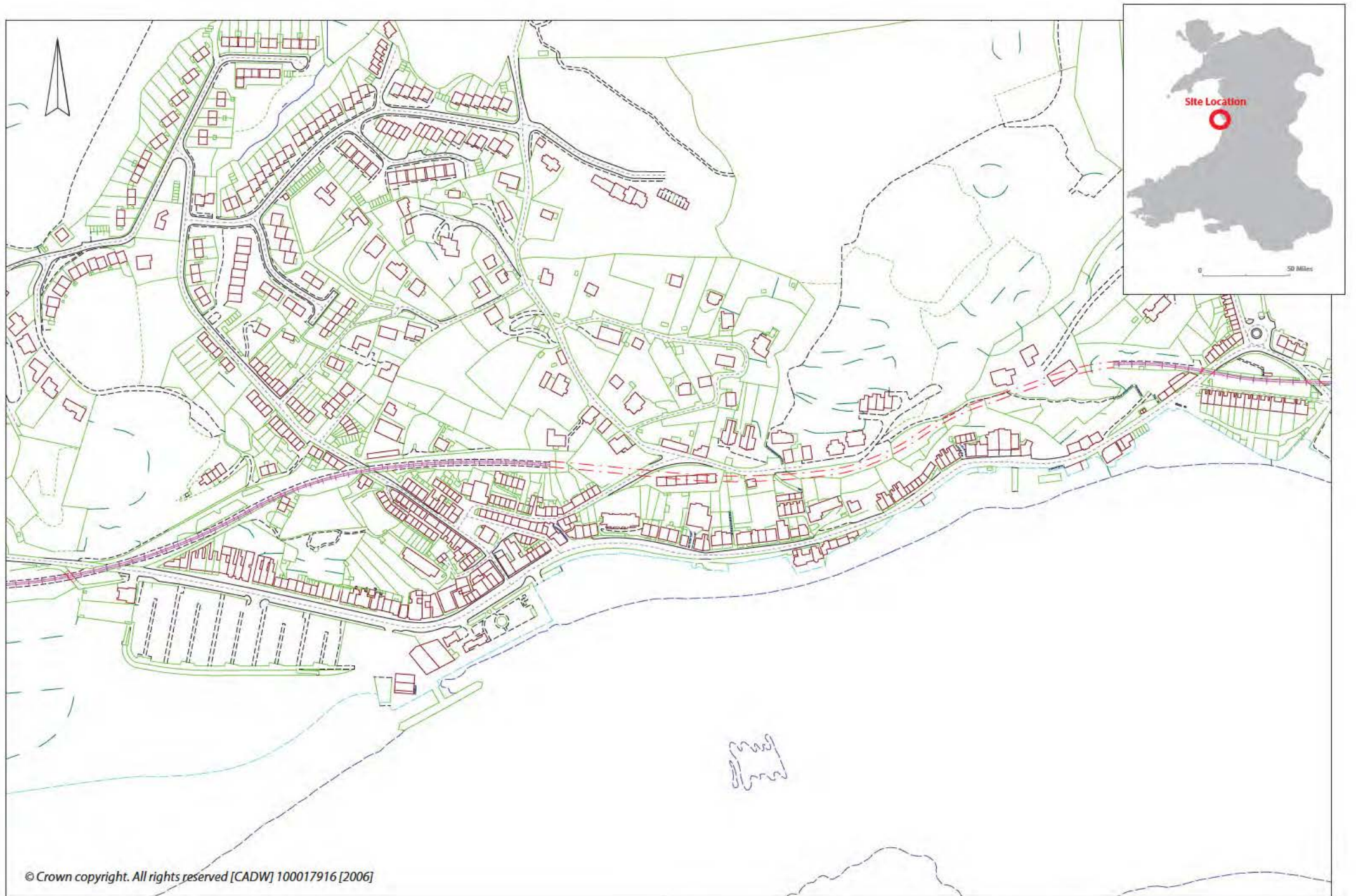


Figure 1: Location Map of Aberdyfi. Scale 1:4000

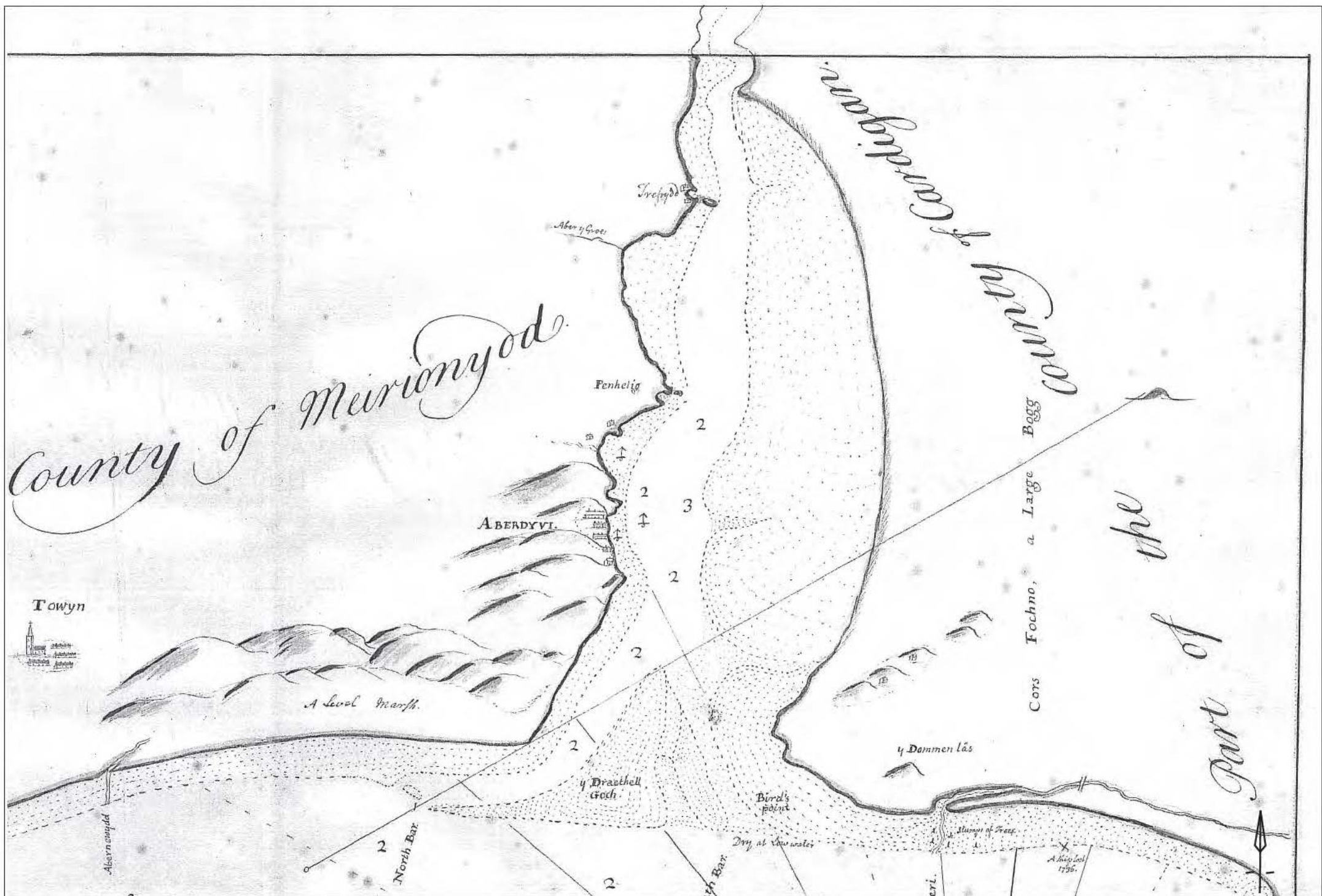


Figure 2: Lewis Morris' Map of Aberdyfi (1748). Not to Scale.

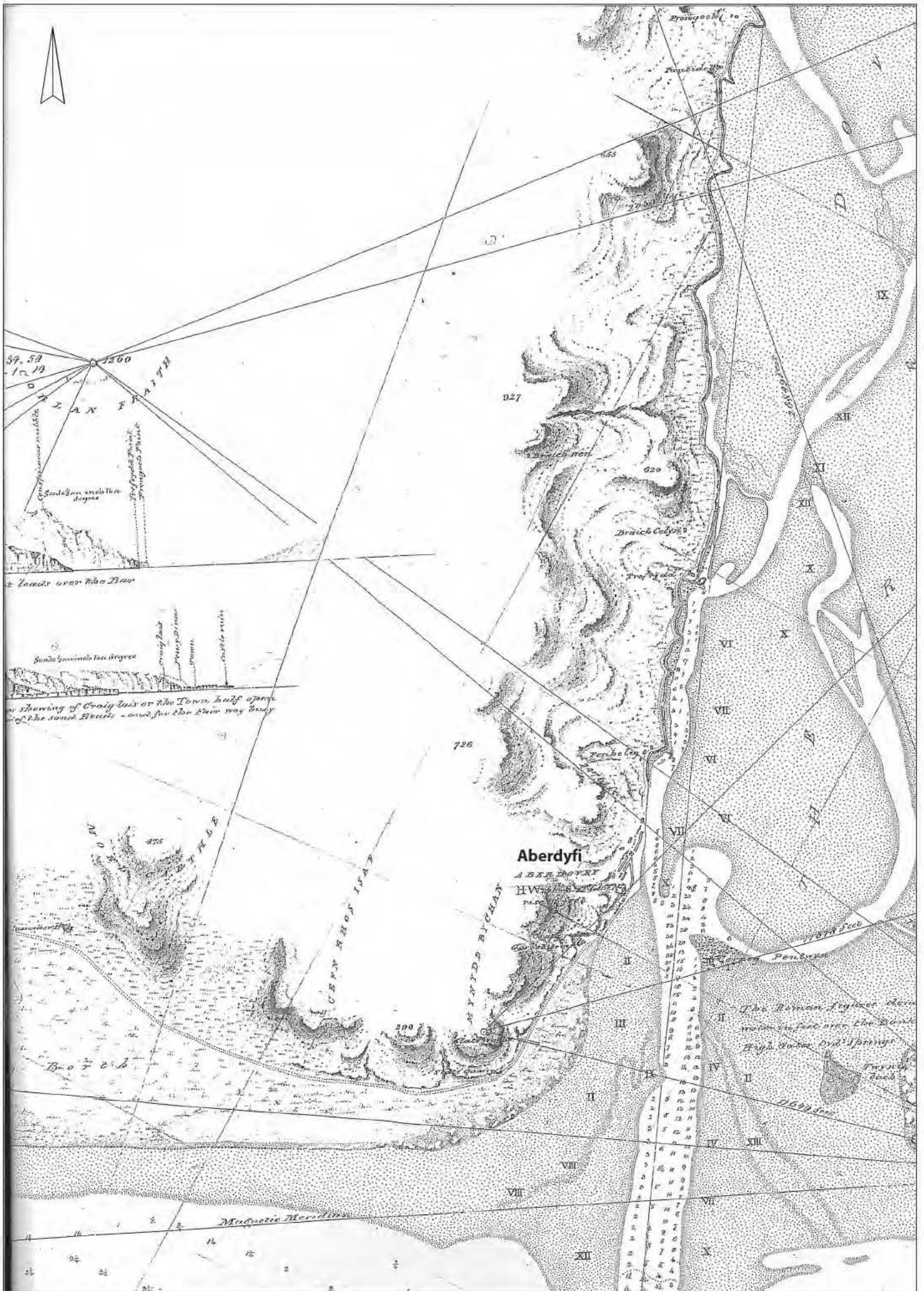


Figure 3: Lt. W. L. Sheringhan RN: Cardigan Bay Sheet III (1835). Not to Scale.



Figure 5: First Edition 25" Ordnance Survey Map of Aberdyfi XLVIII.6 (1888). Scale 1:4000

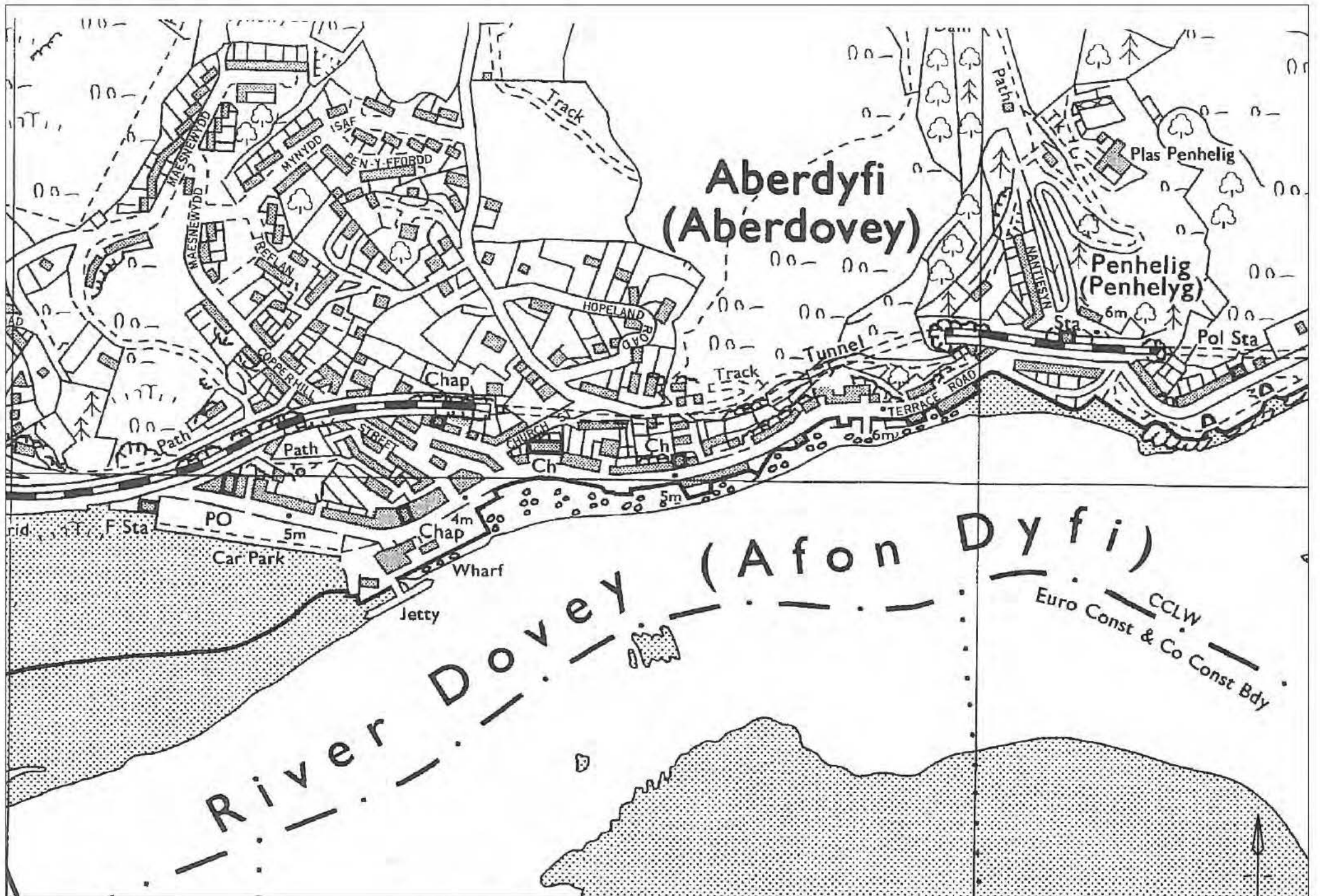


Figure 6: 1:10000 Ordnance Survey Map of Aberdyfi SH69NW (1981). Scale 1:4000



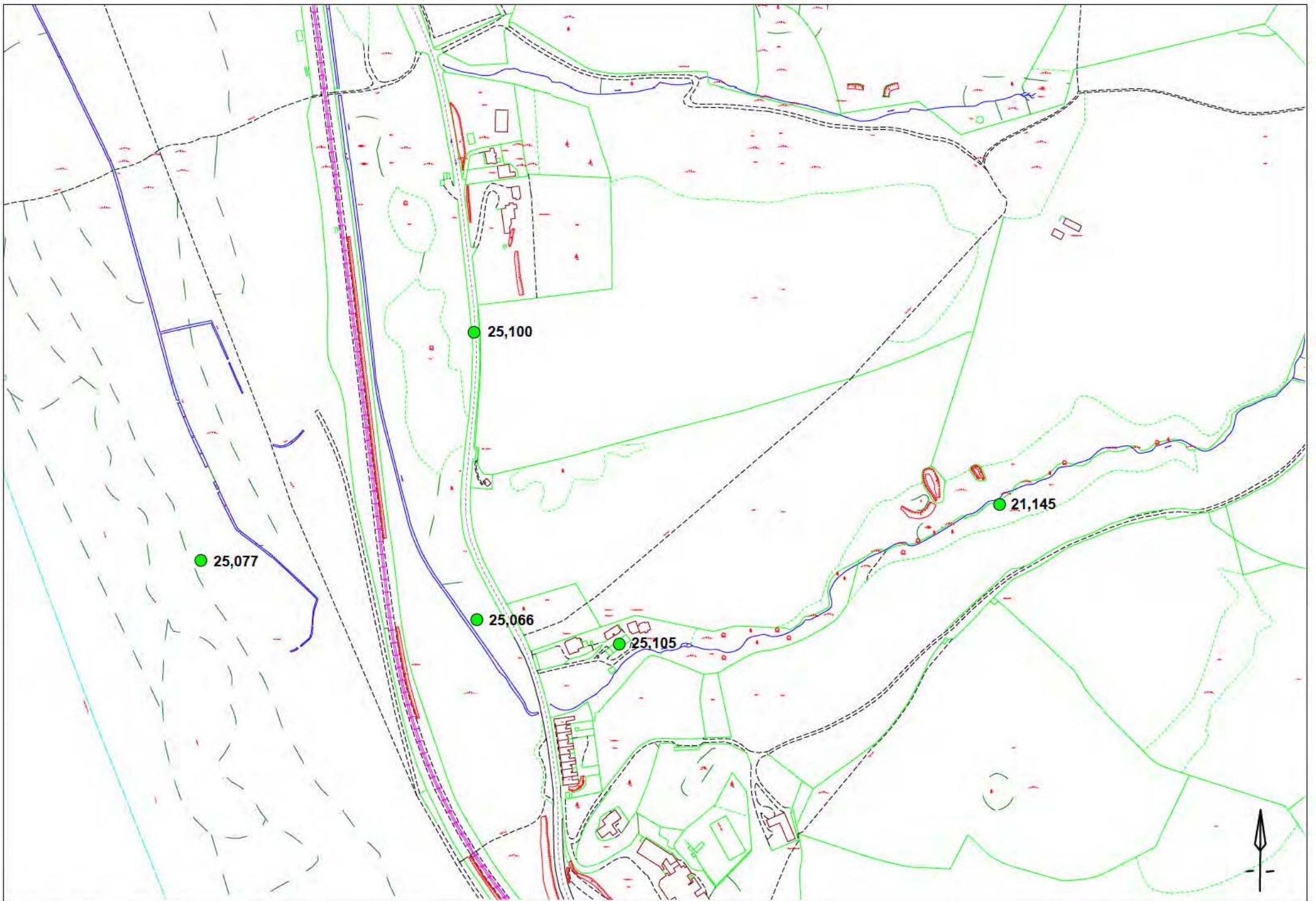


Figure 7: Location of Sites Discussed in Gazetteer: Aberdyfi. 1:5000 © Crown copyright. All rights reserved [CADW] 100017916 [2006]

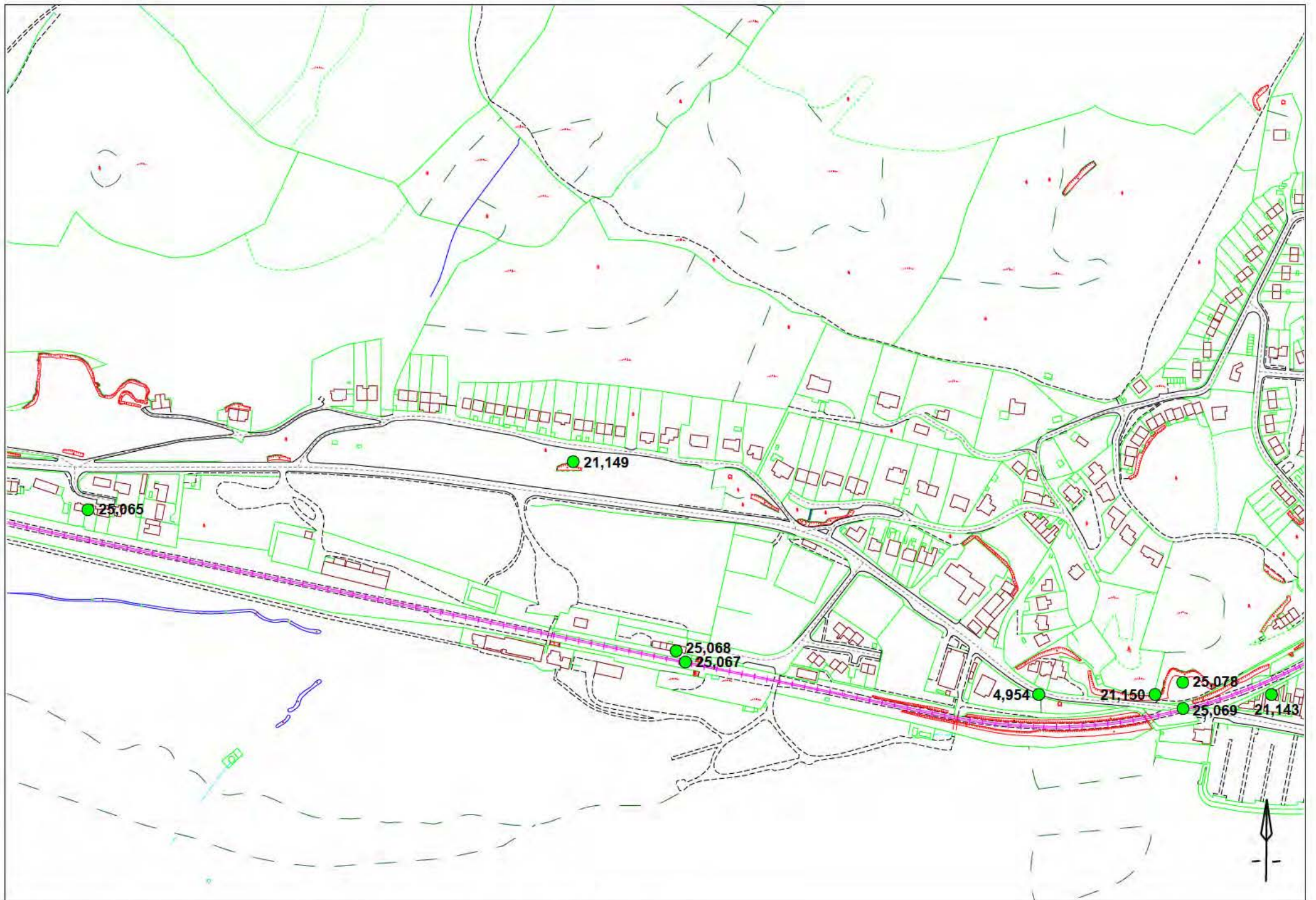


Figure 8: Location of Sites Discussed in Gazetteer: Aberdyfi. © Crown copyright. All rights reserved [CADW] 100017916 [2006]



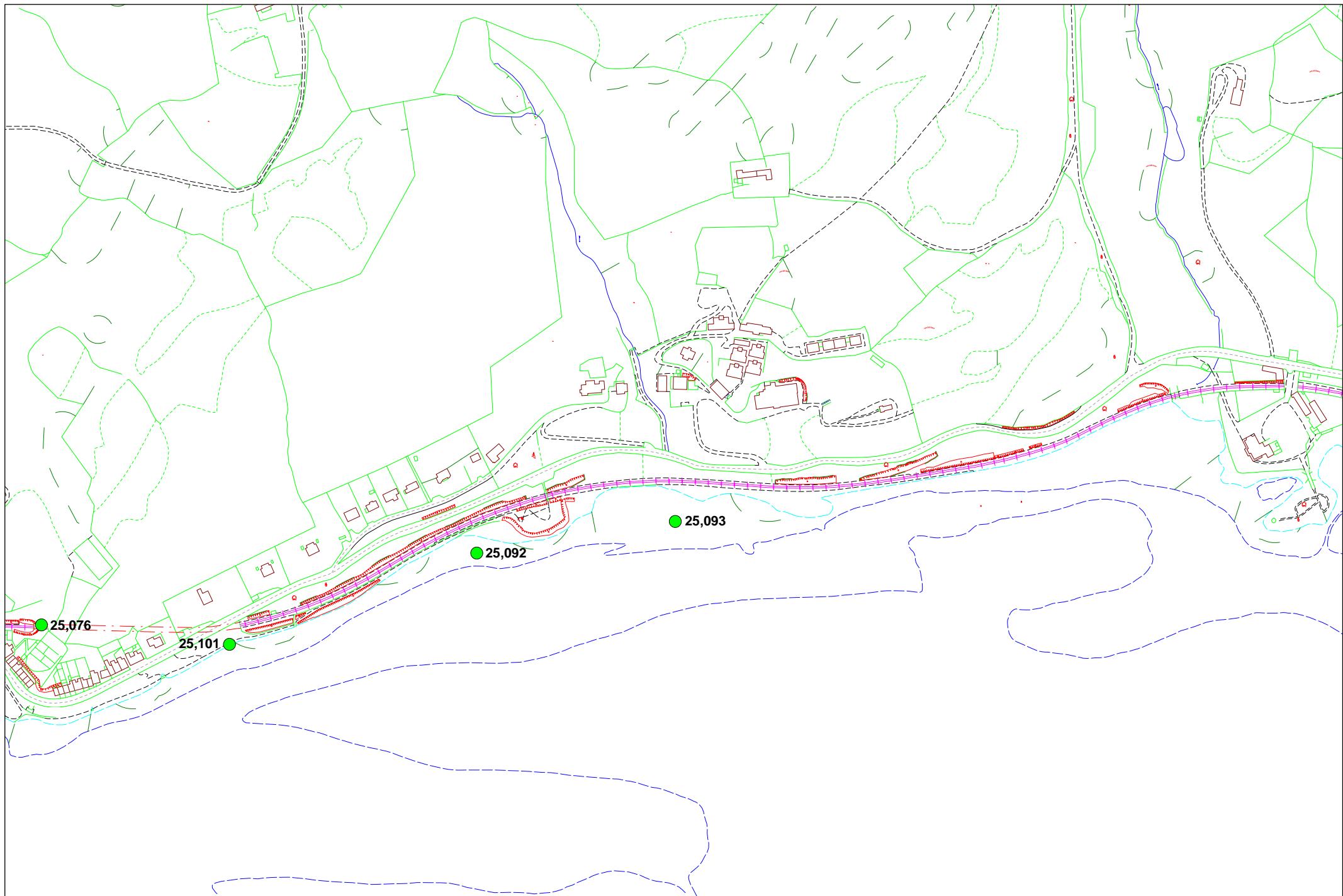


Figure 10: Location of Sites Discussed in Gazetteer: Aberdyfi. 1:5000 © Crown copyright. All rights reserved [CADW] 100017916 [2006]



**Plate 1: Aberdyfi: View of Pier**



**Plate 2: Aberdyfi. View of Vernacular Buildings**



**Plate 3 - Aberdyfi: View of Aberdyfi from Craig Penhelyg 1885**  
(Source: Gwynedd Archives)



**Plate 4 - Aberdyfi: View of Aberdyfi Harbour 1891**  
(Source: Gwynedd Archives)



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