

A470(T) DOLWYDDELAN TO  
PONT-YR-AFANC IMPROVEMENT

ARCHAEOLOGICAL RECORDING

Report number : 345

G1583

Prepared for

HALCROW

WYN THOMAS PLC

NOVEMBER 1999  
(revised December 1999)

Ymddiriedolaeth Archaeolegol Gwynedd  
Gwynedd Archaeological Trust



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# **A470 DOLWYDDELAN TO PONT-YR-AFANC IMPROVEMENT**

## **ARCHAEOLOGICAL RECORDING (G1583)**

### **1. INTRODUCTION**

Gwynedd Archaeological Trust were contracted by Wyn Thomas plc to carry out a program of archaeological recording work in advance of the A470 Dolwyddelan to Pont-yr-Afanc road improvements. Twenty sites were identified in the environmental statement as being in need of recording.

### **2. METHODOLOGY**

Two levels of recording were required; 12 sites were identified for basic recording and a further 8 sites for detailed recording. Site locations are shown on Figs. 1-3.

#### **2.1 Basic recording**

A photographic survey consisting of general views and specific views of features of interest was undertaken for each of the sites. All photographs were taken on both Kodak Tmax 400 pro 35mm monochrome and Kodak Gold 200 35mm colour film and printed at 6 ins x 4 ins. The prints were labelled and catalogued.

A written description of each site was also produced.

#### **2.2 Detailed Recording**

A full photographic survey was carried out for each of the 8 sites designated for detailed recording. This involved a complete record of all accessible internal and external elevations as well as features of particular interest. The photographs were taken, where possible, using a levelled camera with the film plane parallel to the face of the feature. A shift lens was used, where appropriate, to reduce parallax distortion. A series of general views was also taken as specified in basic recording. The detailed photography utilised Kodak Tmax 400 pro monochrome 35mm film, printed at 6 ins x 4 ins. The prints were labelled and catalogued.

A measured survey was undertaken at sites 74, 79, 86, 88, 89, and 91. The surveys at sites 74, 79, 86 and 91 were based on the 1:500 plans provided by Wyn Thomas plc (surveyed by Longdin and Browning Ltd 1992) with additional offset surveys. A full total station survey was carried out of all features to be affected by the road on and around sites 88 and 89. Additional relevant details were added to the total station survey from the 1:500 plan.

A detailed written description was produced for each site.

#### **2.3 Project Archive**

A total of 610 photographs were taken during the basic and detailed recording. All prints were labelled, catalogued and retained along with the field notes, plans and digital survey data in the project archive at Gwynedd Archaeological Trust, Craig Beuno, Garth Road, Bangor, Gwynedd, LL57 2RT. A selection of general photographs has been included in the report. Photographs of several sites were not included because the sites were either obscured by vegetation or were of a minor character and could not be adequately portrayed photographically.

#### **2.4 Recommendations for further mitigatory measures**

Recommendations for further mitigatory measures are made for six sites. Watching briefs are recommended for five of the six sites. Preservation of certain features *in situ* is recommended for the other site.

An archaeological watching brief is defined by the Institute of Field Archaeologists as a formal programme of observation and investigation conducted during any operation carried out for non-

archaeological reasons within a specified area or site where there is the possibility that archaeological deposits may be disturbed or destroyed. The programme will result in the preparation of a report and an archive.

In practice, a watching brief allows an archaeologist to record archaeological deposits that could not be recorded in advance of the development. This entails recording features, either photographically or by written and drawn description, as they are exposed during works on the specified site.

The watching brief will also allow the archaeologist to identify any deposits, features or sites that are too extensive or important to be adequately recorded during the watching brief and to make further recommendations for their recording and excavation.

## 2.5 Survey Conditions

Several sites were heavily overgrown with 1.0m to 1.5m high bracken. Assessment and basic recording of sites 85, 90, 95 and 97 was somewhat impeded but it was felt that sufficient detail could be recorded.

Detailed recording of sites 86, 88 and 89 could not be carried out without clearance of bracken. The bracken had not yet reached its potentially carcinogenic spore-producing phase so clearance was undertaken using a brushcutter.

The weather was variable with several days of light cloud cover and two days of torrential rain.

## 3. RESULTS

### Site 73. Tip - Prince Llewelyn Quarry SH74505281. Basic recording

The waste tips associated with Prince Llewelyn Quarry extend as far as the road at this point and have been partially landscaped to form the grounds of a garage and filling station. The area to be affected by the road improvements consists of a rough slate revetment wall with levelled slate waste above it (Fig 4). The wall stands to a height of 2.2m. The lower 1.6m stands close to vertical and the upper part is battered back. The masonry is irregular and has not been built to a good face. The 8m length of wall and tip adjacent to the road has been landscaped but the wall beyond this is in woodland and appears to be untouched. There is a major collapse in the revetment wall within the woodland at a point 18m from the road. The site was photographed and the photographs have been retained in the archive.

*Recommendations for further mitigatory measures: none*

### Site 74. Tramway, now footpath SH74505272C. Detailed recording

A section of the well-preserved raised tramway running across the river from Prince Llewelyn Slate Quarry will be affected by the road improvements. The tramway is a linear raised feature running for approximately 200m across the valley bottom linking Chwarel y Fedw to Prince Llewelyn Quarry. Slate piers carry the tramway across the immediate floodplain of the Afon Lledr thus avoiding both flooding and steep gradients. The river crossing itself has been replaced by a modern metal bridge. The tramway has now been converted to a footpath and a narrow tarmac path runs along its centre.

The tramway adjacent to the road (Fig. 4) is revetted on both sides by a 1.0m high wall constructed from poor quality slate, much of which has decayed into a very soft product. The road revetment to the north-east appears to be of a similar construction and no definite straight joint or discontinuity could be seen between the two walls. There is however a semi-collapsed area of facing close to the road where the wall changes direction somewhat abruptly which may mark the point where the two walls meet. The raised area of the tramway is 2.6m wide. A modern field access has been constructed at a point 21m from the road by slightly reducing the height of the revetment walls and adding rough ramps to both sides of the tramway.

The revetment walls were photographed in 2m sections between the field access and the road. The photographs were catalogued and have been retained in the archive. Plate 1 shows a view along the tramway from the road.



*Recommendations for further mitigatory measures:* Watching brief in order to record any constructional details

**Site 79 Remains of possible culvert or bridge SH75205360. Detailed recording**

62253

A watercourse or leat runs from the remains of Rhiw Gôch slate quarry to the Afon Lledr and passes under the road via a culvert (Fig. 5). The majority of the edge of the leat is flanked by drystone revetment walls suggesting that it is entirely artificial as opposed to a modified natural stream. The culvert under the road is a recent pre-cast concrete construction. The leat walls adjacent to the culvert were also rebuilt at this point. A concrete slab and the remains of a gabion are also visible in the bed of the leat. Elsewhere the original masonry is standing to a height of around 0.9m. The area to the north-west of the road has been extensively landscaped and an incline from the quarry has been converted into a track.

The leat walls to the south-east of the road have been badly eroded in places resulting in several serious collapses. Part of a raised slate pathway can be seen adjacent to the leat 24m to the east of the road. This is standing to a height of 0.5m and is 1.6m wide. It is flagged with large slate slabs running the entire width of the path. This can be traced for 16m at which point it appears that the path was carried towards and presumably over the river on stone piers.

A well-defined circular hollow, 5m in diameter, was also recorded in the field to the south of the leat. This appeared to be modern but its function remains unclear.

The road improvements will have an impact on the modern culvert and rebuilt masonry but they should not affect much of the original masonry. A detailed photographic record was made of the culvert and surrounding masonry and more general views were taken of the nearby features. The photographs were catalogued and have been retained in the archive.

*Recommendations for further mitigatory measures:* Watching brief.

**Site 81 Sarn Helen - Roman Road. SH75375384 Basic recording.**

The minor road leaving the A470 at this point is thought to follow the line of Roman road. The present road was examined in detail and no earlier features could be seen. The surface is clearly modern and all of the adjoining walls etc appear to be of 19th century date.

*Recommendations for further mitigatory measures:* Watching brief to record any buried remains of the Roman road.

**Site 83 Pont-y-Pant SH75565388 Detailed recording.**

61255

This bridge was formerly a listed building but was later removed from the list presumably as the original structure has largely been replaced by two steel girders and a reinforced concrete roadway (Plate 2). The stone piers are all that remain of the earlier bridge and these will not be affected by the road improvements. General photographs were taken of the bridge but no further recording was undertaken.

*Recommendations for further mitigatory measures:* none

**Site 84 Old stone wall SH75845395 Basic recording**

62254

This site was described in the assessment as a 'good stone wall meeting the road'. There was no wall fitting this description at the map reference. A wall runs to within 14m of the north side of the road and terminates at the given map reference and a wall runs up to the south side of the road. It is likely that

both walls will be affected by the road improvements. A photographic record was therefore made of both walls.

The surviving northern wall is 0.9m thick and stands to a height of 3.0m on a very steep slope. The end of the wall is battered back. The wall continues up the slope for close to 40m but has completely collapsed in several places. The masonry consists of large slate slabs built to a good face. Two trees and a barely perceptible bank adjacent to the road may mark a continuation of the line of the wall to the road. The stone from this section may have been reused in the present road wall.

The southern wall stands on a 45° slope above the river and runs for 6.5m in a southerly direction before terminating above a close to vertical drop. It stands to a height of 0.9m, is 0.6m wide and as such can be seen as a fairly typical 19<sup>th</sup> century field wall. The masonry consists of large slabs of slate forming substantial stretchers with more irregular blocks utilised as through stones. Some upright coping stones survive on the central part of the wall. Only the western side was photographed as the eastern side stands on top of a precipitous drop and was totally obscured by vegetation and a fallen tree. The photographs were retained in the archive.

*Recommendations for further mitigatory measures: none*

**Site 85 Raised area of possible interest.** SH75935397 Basic recording

62257

About 50% of this area was covered in bracken and gorse making detailed assessment difficult. The clear areas consisted of bedrock with a thin covering of turf and lichens. No archaeological features were present here. Several paths were cut through the overgrown areas and these and the area visible beneath the bracken fronds were examined. No archaeological features were detected. Further assessment was beyond the scope of this project, as this would have entailed a programme of complete clearance of the bracken. This limited assessment should however have identified any significant archaeology. General photographs were taken of the area and retained in the archive

*Recommendations for further mitigatory measures: none*

**Site 86 Ruined buildings or sheep pens and enclosure.** SH76005400 Detailed recording

A complex of sheepfolds, centred around an earlier agricultural building, stand on the north side of the A470 opposite Caedu (Plate 4 and Fig. 6). A detailed survey was undertaken based on offsets from known points taken from the Longdin and Browning 1:500 plan. The results are shown on Fig. . Each of the six compartments was allocated a separate letter in order to simplify the descriptions and photographic records.

Compartment A is a former agricultural building with internal dimensions of 5.5m x 4.4m and a 0.95m wide and 1.65m high entrance in the north-eastern wall. The masonry in this building is much more substantial than in the other sheepfolds. The walls stand to a height of close to 2.0m and are constructed from large blocks of stone interspersed with smaller slate slabs. The end walls are 0.9m thick and the side walls 0.7m thick. The southern corner of the building has been partially destroyed, probably by a road traffic accident.

Compartment B is a somewhat irregular enclosure with dimensions of 4.6m x 5.6m and walls standing to a height of 1.4m to 1.6m. The south-eastern wall abuts the outer face of building A. There are two openings in the north-eastern wall separated by a block of masonry added to the end of a field wall. A narrow 0.6m wide entrance leads into compartment C. The masonry style is similar to that of compartments C and F, i.e.; the walls are 0.7m thick and are constructed from small, flat, irregular pieces of slate. A small niche or alcove (dimensions: width 0.72m, height 0.5m, depth 0.5m) standing 0.75m above ground level is visible in the north-western wall.

Compartment C is a rectangular structure with dimensions of 6.0m x 4.2m and walls standing to a height of 1.6m. The south-western wall abuts building A. A blocked and partially collapsed sheep run

(width 0.85, height 1.05) and a blocked doorway (width 0.85m) can be seen as straight joints in the masonry of the north-eastern wall.

Compartment D is an irregular enclosure formed by a steep natural slope at the west and the outer face of compartments A, C and E to the south-east. A curving wall with a 1.1m wide entrance forms the north-eastern wall.

Compartment E is a three sided enclosure or yard with dimensions of 9.4m x 6.5m. The walls are 0.5m thick, stand to a height of 1.5m and are capped by upright coping stones.

Compartment F is a small rectangular enclosure or pen with dimensions of 2.2m x 1.6m built into the western corner of compartment E. The entrance is in the north-eastern wall and is 0.8m wide.

A 3m wide irregular platform to the north-west of compartment C could be the remains of an earlier building. This area was however very overgrown and further assessment was not possible.

All inner and outer faces were photographed as specified in section 2.2 (above). The photographs were catalogued and have been retained in the archive.

*Recommendations for further mitigatory measures:* Watching brief on platform to the north-west of compartment C.

#### **Site 87 Old stone wall.** SH76255402 Basic recording

A substantial drystone boundary wall runs from the south side of the A470 at SH76125403. The wall runs downslope in a south-easterly direction for 60m before turning towards the east and north and following the 107m contour along the valley bottom for 120m.

The wall runs through almost impenetrable undergrowth for much of its length. The wall was accessible in the valley bottom at about SH76255402 (Plate 5). At this point the wall stands to a height of around 1.6m and is 0.6m wide. The masonry is built to a rough face from large sub-rounded stones. It appears that little of the wall apart from that immediately adjacent to the road, will be affected by the road improvements. Photographs were taken at the point where the wall abuts the road wall but the masonry was almost completely obscured by dense foliage.

*Recommendations for further mitigatory measures:* none

#### **Sites 88 and 89 Track and Ruined buildings** SH76515411/SH76595410 Detailed recording

Sites 88 and 89 form part of a larger complex of quarry workings and buildings (Fig. 7). There are no known written records of this quarry and it is not known what was extracted. There are however large amounts of quartz around the workings. It is unlikely that quartz extraction would be financially viable so it can be assumed that a mineral associated with the quartz veins was being extracted. The quarry itself stands 200m north of the road. An incline, terminating in a large stone chute, stand beneath the quarry. One or possibly two trackways run from this point to site 88 and a raised barrow run runs to site 89. None of the above features will be affected by the road. This part of the plan was compiled from the earlier assessment report and the 1:500 Longdin and Browning plan.

Site 88 was identified in the original assessment as a track running to the quarry. It was heavily overgrown with bracken making recording impossible. The lower part of the site was therefore cleared using a brushcutter. The upper part of the trackway could not be cleared in the time available and was therefore not recorded.

The track runs from the A470 to the structures immediately beneath the quarry. Its orientation suggests that it runs to a level area immediately below the chute. The track is terraced into and built out onto the hillside to a width of 3m to 3.5m. The lower side retains a few traces of revetment walling. A further

length of revetment walling 3m to the north-west of the track may represent the lower side of a further narrow trackway, possibly running to the trackway identified below the quarry. This is in poor condition making interpretation difficult.

A small (5m x 5m) revetted platform adjoins the southern side of the trackway. This may have originally contained a building as the top of the platform is entirely obscured by rubble. A large orthostatic stone in excess of 2m in length stands at the south-western end of the platform.

Site 89 was heavily overgrown with bracken making recording impossible. The site was therefore cleared using a brushcutter.

The raised barrow run from the quarry runs to a drystone ramp or small incline (Plate 6). This structure stands to a height of 2.0m at its northern end and has dimensions of 9.3m x 2.9m. The northern half of the structure originally formed a level platform. This was subsequently partially dismantled in order to build a rectangular shelter (Plate 7). The southern half of the structure forms a steep ramp running down towards the road at an angle of about 40°. The end of the ramp runs into a 13m long chute, now entirely grassed over.

A loading bay with dimensions of 4.9m x 5.9m has been cut into the slope below the ramp (Plate 8). A drystone revetment wall standing to a height of 1.6m forms the back wall of the bay.

Three grassed over hollows can be seen at the western side of the ramp. The northernmost is circular with a diameter of 3.25m and is cut 0.6m into the ground. The sides of the feature are close to vertical and may retain remnants of stone facing. A subrectangular platform with dimensions of 3.6m x 3.4m has been cut into the slope just to the south of this. A further 1.6m diameter hollow can be seen in front of the platform.

This sequence of hollows may be the remains of processing floors or other structures where products from the quarry were crushed and separated. Further work is needed in order to define further these structures. Another 1.9m diameter hollow stands just to the west of the ramp. A further, larger hollow, adjacent to the loading bay may be a natural depression partially modified by material forming the western side of the bay.

The later shelter cut into the stone ramp presumably post-dates the other structures, as the ramp and therefore the barrow run, chute and loading bay would have been rendered inoperative by its construction.

The shelter is rectangular with internal dimensions of 3.5m x 2.5m. The walls are drystone, stand to a height of 1.5m and are 0.7m thick. This feature is probably a sheep pen.

Site 88 will not be substantially damaged by the road but site 89 will be entirely destroyed. Both sites were planned using a total station and all inner and outer elevations were photographed in detail. The photographs were catalogued and have been retained in the archive.

*Recommendations for further mitigatory measures:* Watching brief on site 89.

#### Site 90 Terracing. SH76815405 Basic recording

62259

The terracing on the hillside adjacent to Maes-y-Coed was examined in some detail. This was found to be entirely natural. A dilapidated, modern corrugated iron sheep pen stands about halfway down the field. General photographs were taken of the area and retained in the archive.

*Recommendations for further mitigatory measures:* none



**Site 91 Four stone piers SH77515403 Detailed recording**

PRN  
62260

Four stone piers stand on a rocky outcrop just to the south of the A470 (Plate 9 and Fig. 8). Three are rectangular in plan and one is subrectangular. All are constructed from a mixture of roughly mortared slate, rhyolite and other stone. Each pier is capped with very coarse unshuttered concrete. The piers form a roughly semicircular alignment.

Pier 1 stands to a height of 2.2m and has dimensions of 1.5m x 1.4m in plan. The north-western corner has begun to collapse and only stands to a height of 1.5m.

Pier 2 stands to a maximum height of 2.0m and is subrectangular in plan with the northern side offset at an angle (dimensions: 1.5m x 2.0m x 1.5m x 2.4).

Pier 3 stands to a maximum height of 2.0m and has dimensions of 1.5m x 2.0m in plan.

Pier 4 stands to a height of about 2.0m and has dimensions of 1.8m x 1.2m in plan. This pier was not fully recorded, as it was inaccessible, standing at the bottom of a steep drop in impenetrable 2m high brambles.

The function of the piers is unclear. They appear to be of industrial origin probably dating from the early part of the 20<sup>th</sup> century. No mineral extraction sites are marked in the vicinity on the 1:10000 OS map and no other compatible structures could be located on the ground. A levelled area of concrete and tarmac on the opposite side of the road could have contained other structures at one time, possibly associated with the quarrying further up the valley.

The piers all stand to different relative heights, pier 3 being the highest. Pier 1 is 0.7m lower than pier 3 and pier 4 is close to 2m lower. Their alignment in a curving path suggests that they may have carried a launder or chute but their relative heights suggest otherwise.

The tops of the piers were covered in a mat of moss, grass and heather. This was cleared from the top of pier 3 in order to look for fixing plates or bolts. Nothing was found. The concrete capping was however very rough suggesting that its function was to make the piers safe after abandonment. It is therefore possible that the structures had been reduced in height by the time they were capped and that they may have carried a chute or launder.

All elevations of piers 1 to 3 were recorded photographically. Only general views were taken of pier 4 due to its inaccessibility. The photographs were catalogued and have been retained in the archive.

*Recommendations for further mitigatory measures:* none

**Site 93 Culvert. SH78205385 Basic recording**

PRN 62260

A very well constructed brick and stone culvert carries a stream beneath the railway embankment and then beneath the A470 at a point 35m to the east of Gethin's Bridge, a Grade II Listed viaduct. The culvert appears to be contemporary with the viaduct, as the masonry styles are similar. The culvert beneath the embankment is 1.20m wide and 1.64 m high. The culvert bed is constructed from pitched stones laid crossways and retained at the entrance by a large stone slab. The walls consist of roughly shaped stone supporting an arching brick roof. The arch is level, in contrast to the sloping floor, and runs into the culvert for 1.8m before stepping up by 0.45m. This is followed by another 2.1m level section. The stepped roof continues throughout the entire 17m length of the culvert. The slope behind the culvert is revetted by 4.5m long wing-walls running at 45° to the ends of the arch.

The water is carried beneath the road in a rectangular stone built culvert, roofed and floored with stone slabs. The opening to the south of the road is 0.85m wide and 1.25m high.

*Recommendations for further mitigatory measures:* Avoidance of damage to culvert under the embankment.

Site 94 Buildings SH78255384C Basic recording

PRN

62262

Two 19<sup>th</sup> century agricultural buildings stand on the south side of the road at Tan-yr-Allt. One building stands almost opposite Tan-yr-Allt farmhouse (Plate 12). This building, cut lengthways into the slope, has dimensions of 5.7m x 4.7m. A small extension stands on the southern side. Steps lead up to a door in the northern gable end and a further door and small window can be seen in the eastern wall. The walls are constructed from roughly mortared stone beneath a modern slate roof.

The other building stands 70m to the west of Tan-yr-Allt and is cut sideways into the slope parallel to the road. Its external dimensions are 9.2 x 5.2m and two windows, now at the level of the road can be seen in the northern wall. A modern 'up and over' garage door has also been inserted in this wall. The walls are constructed from roughly mortared stone beneath a modern corrugated iron roof. The rear of the building is totally overgrown and inaccessible. The remains of a recent pigsty abut the southern wall.

Another pigsty stands in the woodland to the south of the road but this could not be located due to heavy undergrowth.

*Recommendations for further mitigatory measures: none*

Site 95 Rocky patch. SH78405381 Basic recording

The landowner reported that a rocky patch in the field to the south of the road was all that remained of a sight terrace and field bank that was cleared around the time that the present road was constructed. A few possible facing stones could be seen amongst the rubble. General photographs were taken and have been retained in the archive.

*Recommendations for further mitigatory measures: none*

Site 96 Track SH78955403C Basic recording

PRN 6 2263

A track runs from the A470 at SH79155401 and runs on an alignment slightly to the north of the road for about 250m before turning uphill into the woods to the north. The only part of the track to be directly affected by the road improvements is at the point where it joins the A470. Photographs were taken at several points along the track allowing it to be seen in a wider context.

The track is now largely disused and overgrown with turf. In places where erosion has occurred the surface can be seen to consist of mixed stones and cobbles probably from field clearance. The track is 3 to 4m wide and is terraced into the hillside. A stone lined watercourse, 0.8 to 1.0m wide, runs down the hill from the railway embankment. The track passes over this via a rather inadequate bridge consisting of a single curving sheet of corrugated iron which appears to have replaced an earlier wooden or stone span. The drain meets the road at SH78835399 where it will be slightly damaged by culverting operations. General photographs were taken of both the track and the drain and have been retained in the archive.

*Recommendations for further mitigatory measures: none*

**Site 97 Remains of sheep pen or building. SH78955399 Basic recording**

PRN 62264

The very overgrown remains of a two compartment sheep pen stand to the south of the road. Enough nettles and brambles were cleared from the site to ascertain its plan and take general photographs.

The easternmost enclosure has internal dimensions of 5.5m x 3.2m and appears to be a reused agricultural building. The walls stand to a height of 1.2m and are 0.8m thick. The drystone masonry consists of large blocks of straight-edged local stone, laid to a good face. There is a 1m wide entrance in the eastern wall. A later, curving wall links the south-eastern corner of the enclosure to a large 4.3m log boulder. The westernmost enclosure abuts the reused building and has internal dimensions of 5.5m x 3.0m. The walls here are less well built and somewhat narrower. The northern walls of both enclosures have been largely destroyed by the present road. General photographs were taken of the site. Much of the building was obscured by dense vegetation and the photographs have therefore been retained in the archive.

*Recommendations for further mitigatory measures: none*

**Site 99 Pont-ar-Lledr Grade II Listed Bridge. SH79675418 Detailed recording**

PRN 62265

The bridge consists of two elliptical arches one larger than the other, with a cutwater between them. It is thought to date from the late 18<sup>th</sup> century (RCAHMW 1959). Only the outer wing walls are to be affected by the road improvements. The inner faces of these were recorded in detail and the catalogued photographs retained in the archive. The outer faces were entirely obscured by vegetation and were therefore not recorded.

The north-eastern wing wall is in good condition but showing evidence of several repairs. The inner face stands to a height of 0.8m and consists of mortared stone with a coping of large flat slabs. The wing wall joins to the later road wall at a point 1.3m beyond a post box set back from the side of the bridge. The end of the opposite wing wall has been destroyed and is now marked by a line of rubble.

*Recommendations for further mitigatory measures: none*

#### 4. SUMMARY OF RECOMMENDATIONS FOR FURTHER MITIGATORY MEASURES

The following table summarises the recommendations for mitigatory measures as described in section 3 (above).

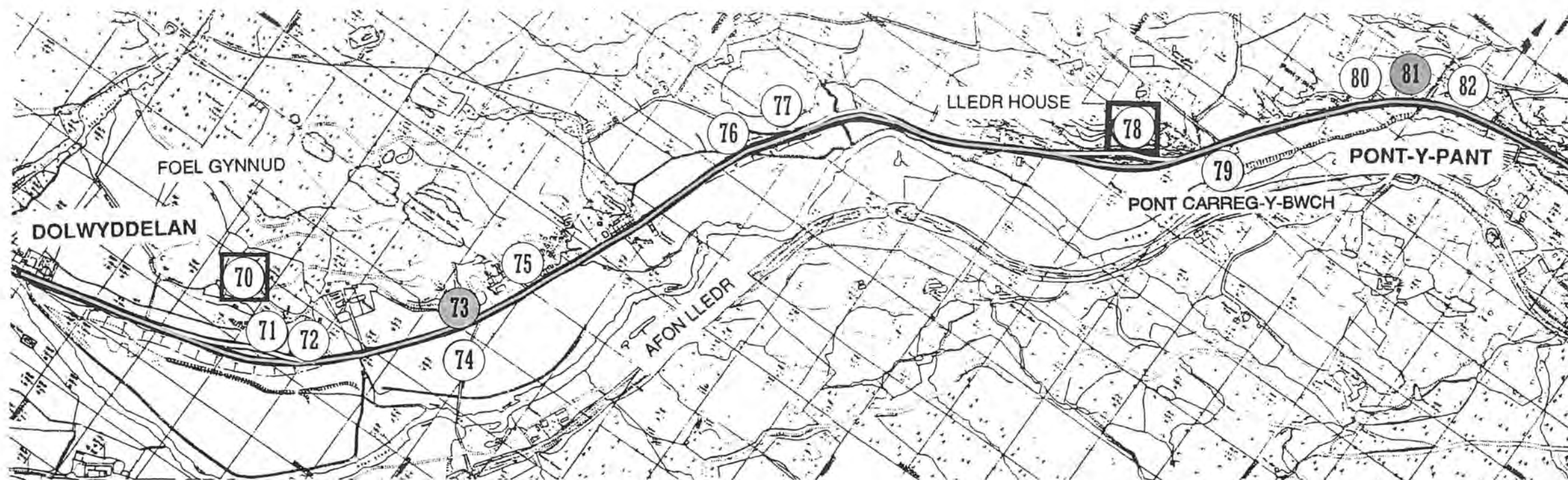
SITE No	SITE DESCRIPTION	FURTHER MITIGATORY MEASURES
73	Tip - Prince Llewelyn Quarry	None
74	Tramway, now footpath	Watching brief in order to record any constructional details.
79	Remains of possible culvert or bridge	Watching brief
81	Sarn Helen - Roman Road	Watching brief in order to record any buried remains of the Roman road.
83	Pont-y-Pant	None
84	Old stone wall	None
85	Raised area of possible interest	None
86	Ruined buildings or sheep pens and enclosure	Watching brief on platform to the north-west of compartment C.
87	Old stone wall	None
88	Track	None
89	Ruined buildings	Watching brief
90	Terracing	None
91	Four stone piers	None
93	Culvert	Avoidance of damage to culvert under the embankment.
94	Buildings	None

95	Rocky patch	None
96	Track	None
97	Remains of sheep pen or building	None
98	Pont-ar-Lledr Grade II listed bridge	None

## 5. REFERENCES

R.C.A.H.M.W., 1956. *An inventory of the Ancient Monuments in Caernarvonshire* Vol. 1 East





#### LEGEND



EXISTING A470(T)

A470(T) IMPROVEMENT



CATEGORY II SITES  
DETAILED RECORDING



CATEGORY III SITES  
RECORDING BY PHOTOGRAPH  
AND DESCRIPTION



SITES ALLOCATED TO OTHER  
CATEGORIES



SCHEDULED AND LISTED SITES  
(BUILDINGS OR STRUCTURES OF SPECIAL  
ARCHITECTURAL OR HISTORIC INTEREST)

87

SITE REFERENCE NUMBERS REFERRED  
TO IN REPORT

Welsh Office

Y Swyddfa Gymreig

A470 DOLWYDDELAN TO  
PONT-YR-AFANC IMPROVEMENT

ARCHAEOLOGICAL RECORDING  
DOLWYDDELAN TO PONT Y PANT

Figure No. 1

Scale As Shown

Sir William Halcrow & Partners Ltd.  
Wyn Thomas & Partners  
Halcrow Fox

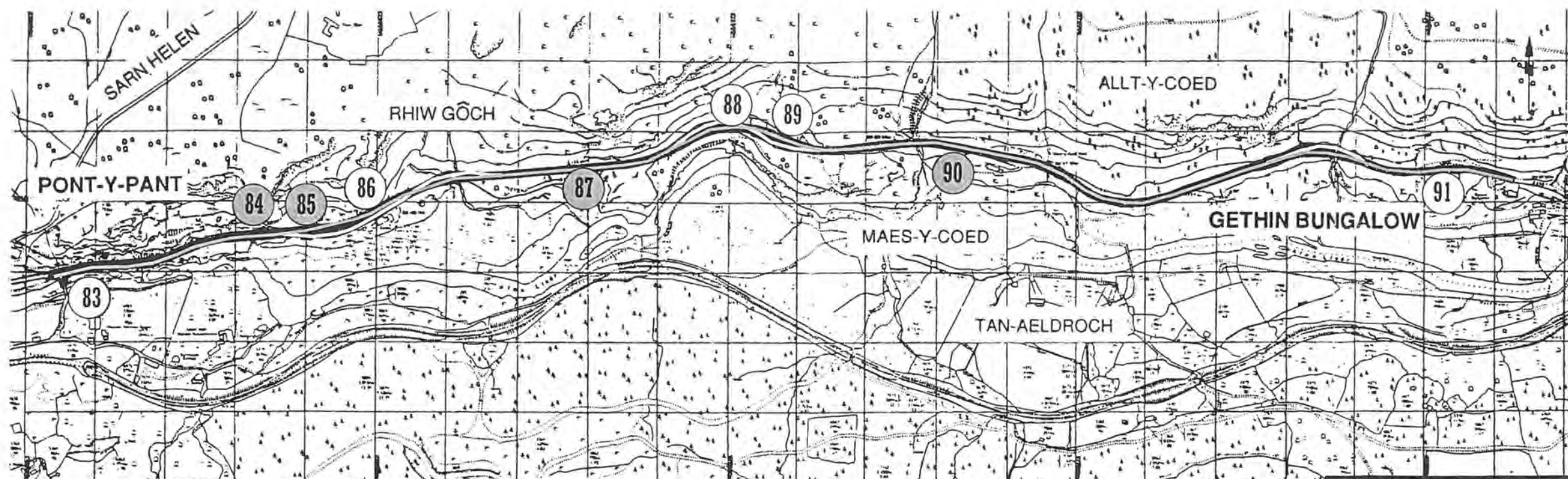
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DETAILED RECORDING
-  CATEGORY III SITES  
RECORDING BY PHOTOGRAPH  
AND DESCRIPTION
-  SITES ALLOCATED TO OTHER  
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-  SCHEDULED AND LISTED SITES  
(BUILDINGS OR STRUCTURES OF SPECIAL  
ARCHITECTURAL OR HISTORIC INTEREST)
-  SITE REFERENCE NUMBERS REFERRED  
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**A470 DOLWYDDELAN TO  
PONT-YR-AFANC IMPROVEMENT**

**ARCHAEOLOGICAL RECORDING  
PONT Y PANT TO GETHIN BUNGALOW**

Figure No. 1.2

Scale As Shown

Sir William Halcrow & Partners Ltd.  
Wyn Thomas & Partners  
Halcrow Fox

Sheet Layout

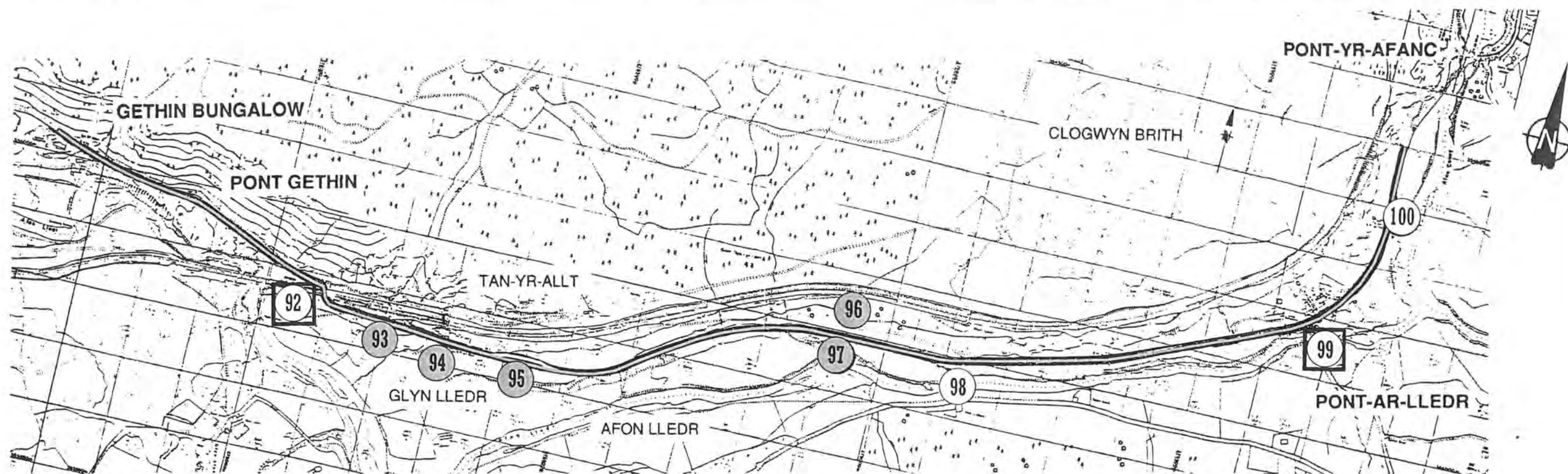


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SITES ALLOCATED TO OTHER  
CATEGORIES



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A470 DOLWYDDELAN TO  
PONT-YR-AFANC IMPROVEMENT

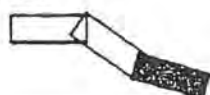
ARCHAEOLOGICAL RECORDING  
GETHIN BUNGALOW TO PONT-YR-AFANC

Figure No. 3

Scale As Shown

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Halcrow Fox

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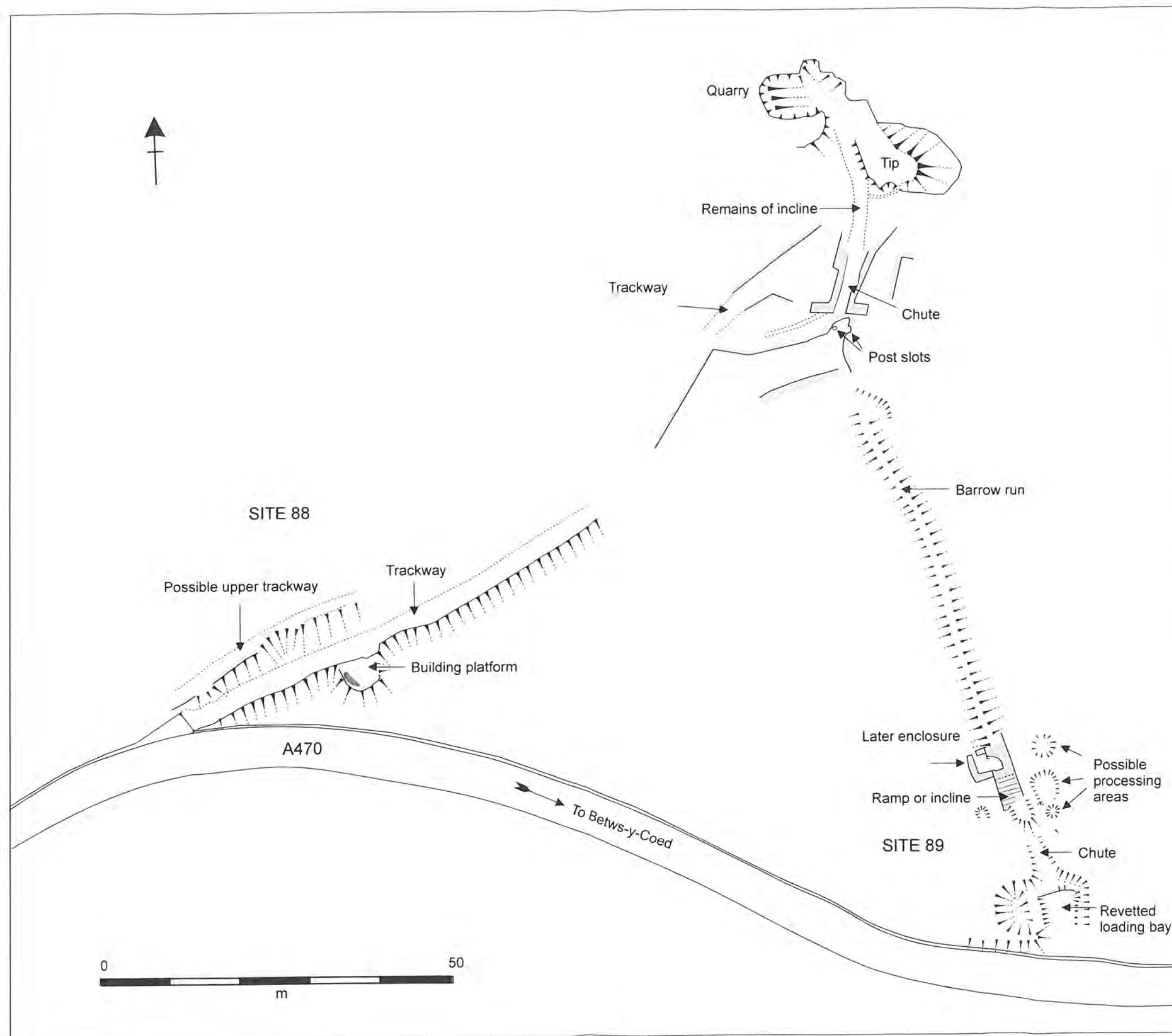


Fig. 7 Sites 88 and 89, quarry workings

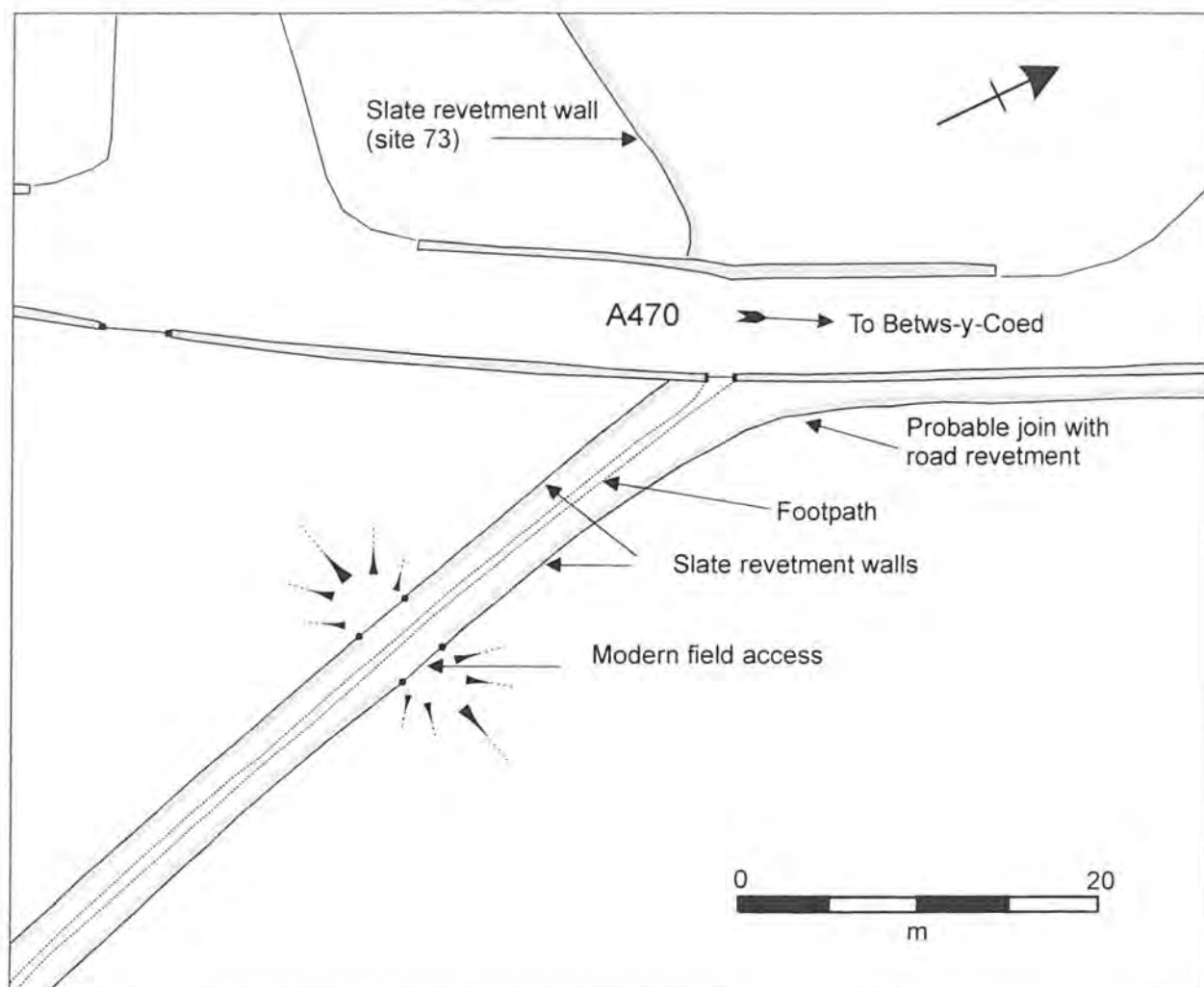


Fig. 4 Site 74, Tramway converted to footpath and location of site 73

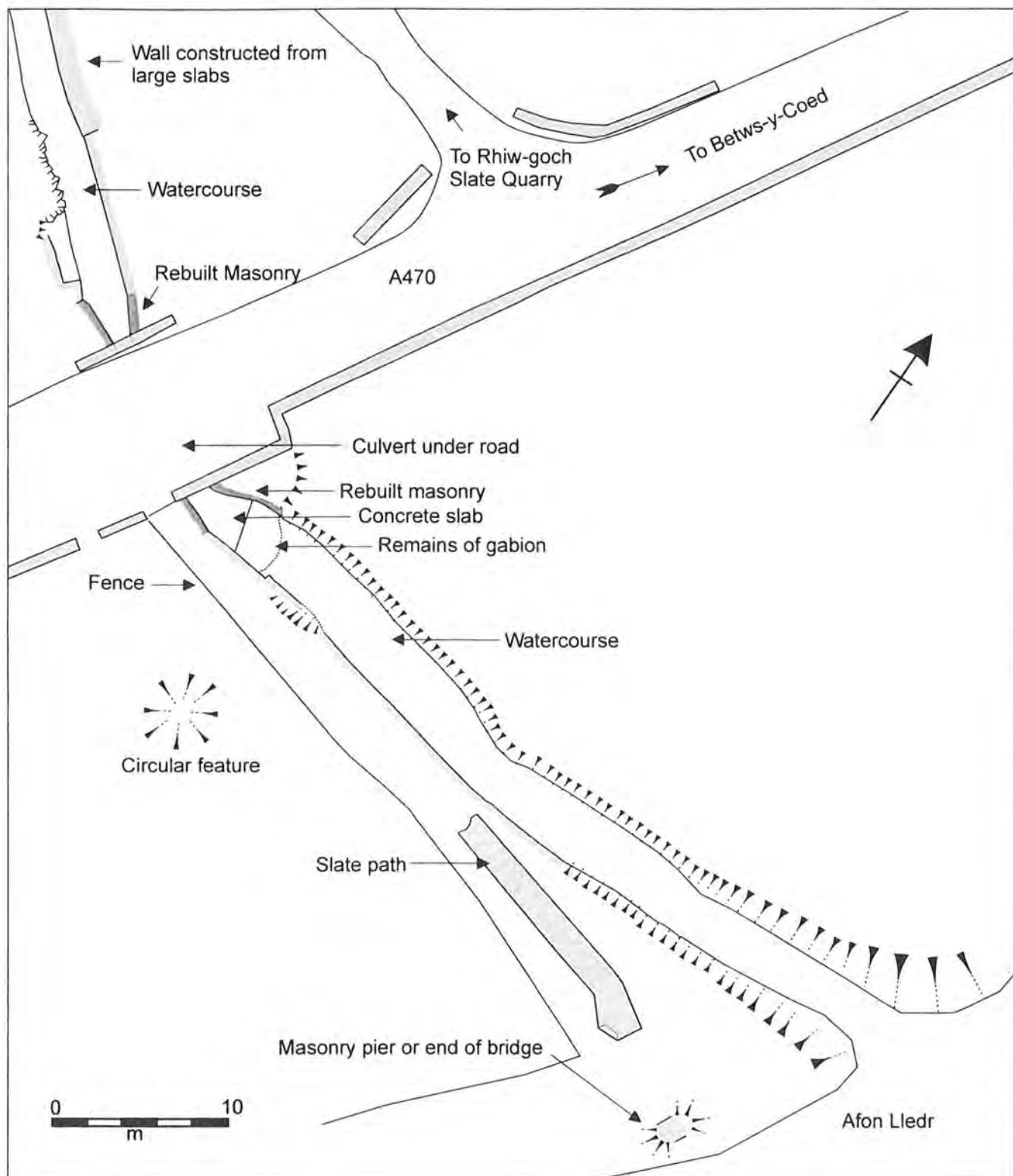


Fig. 5 Site 79, remains of culvert and bridge

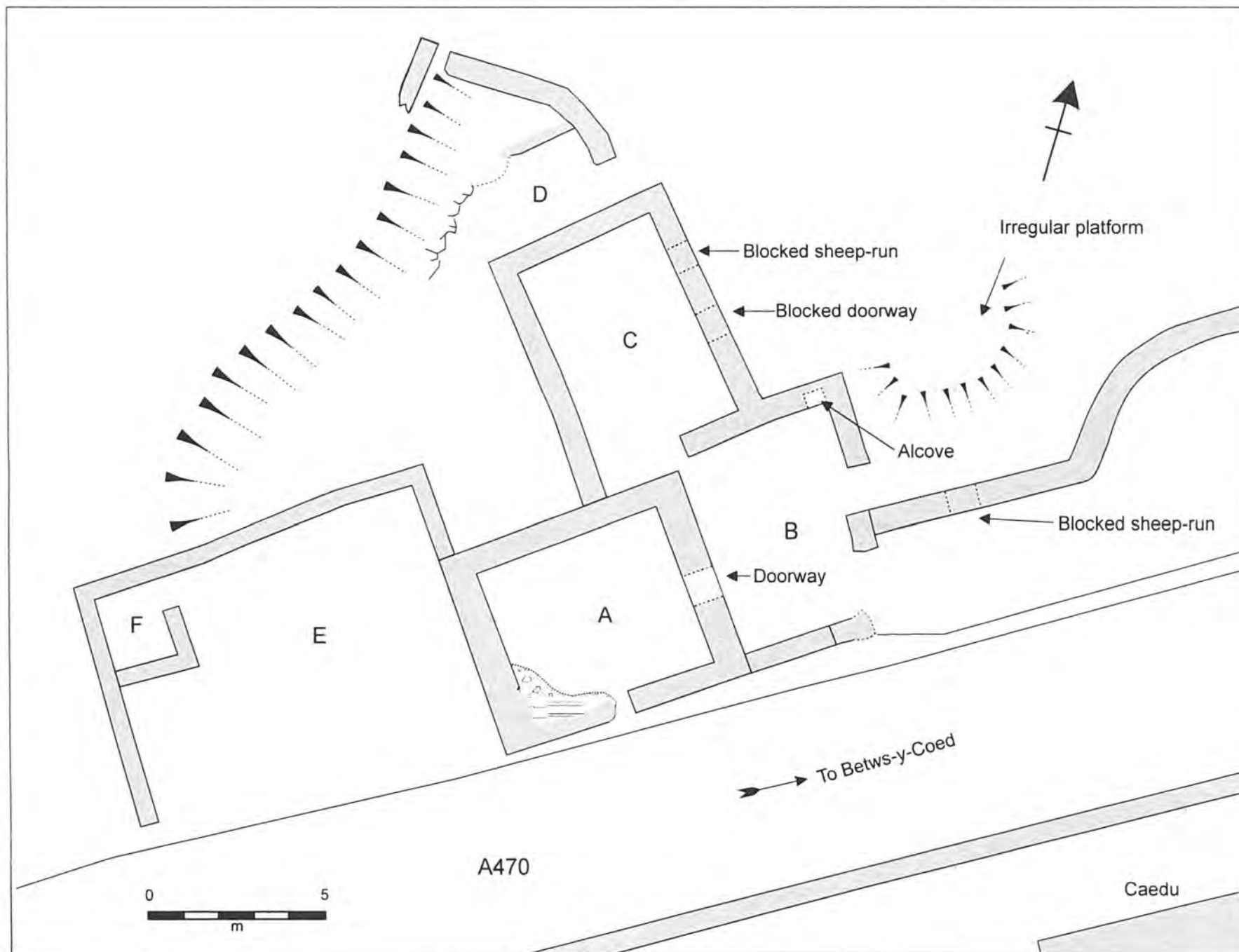


Fig. 6 Site 86, ruined building and sheep pens





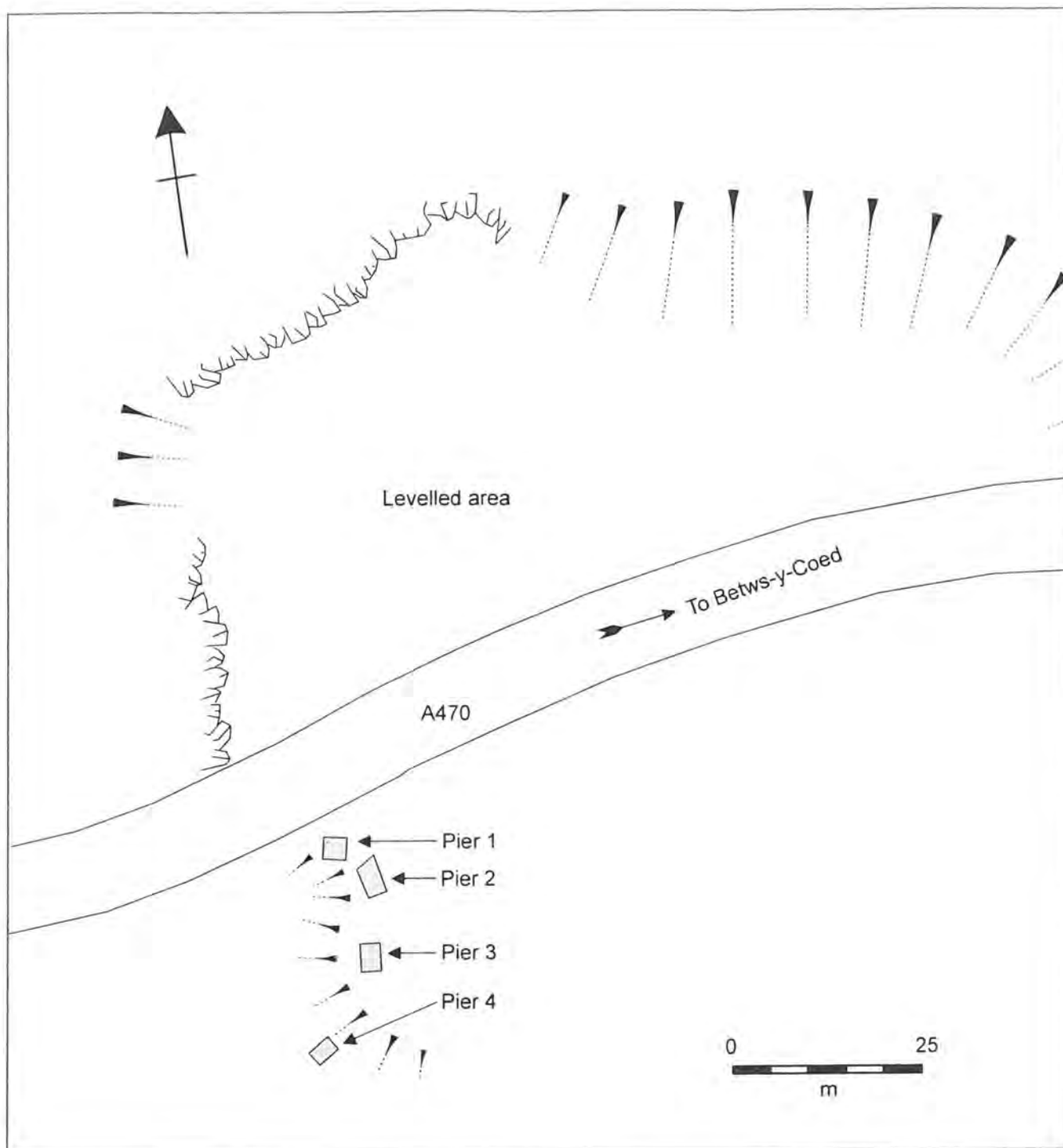


Fig. 8 Site 91, four stone piers

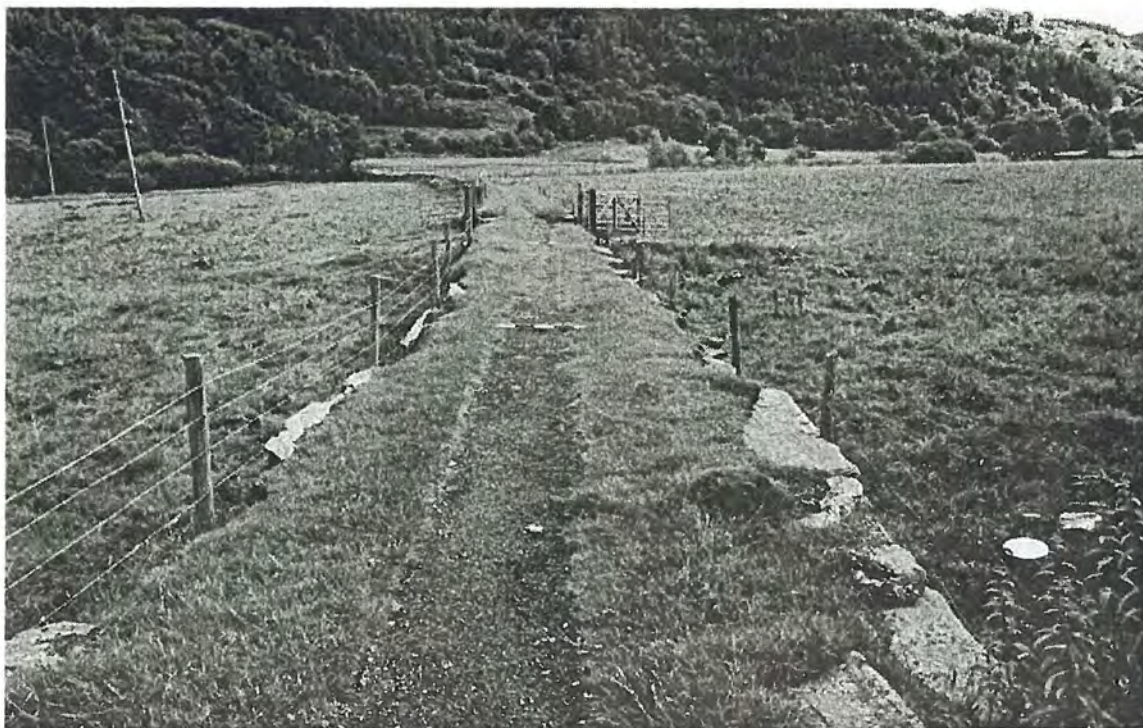


Plate 1. Site 74; view along tramway from the road



Plate 2. Site 83, Pont-y-Pant, showing rebuilt roadway





Plate 3. Site 84, northern wall



Plate 4. Site 86, sheep pens etc





Plate 5. Site 87, wall in valley bottom



Plate 6. Site 89, stone ramp





Plate 7. Site 89, barrow run and later shelter.



Plate 8. Site 89, loading bay





Plate 9. Site 91, piers 1 to 3



Plate10. Site 93, Culvert beneath the viaduct



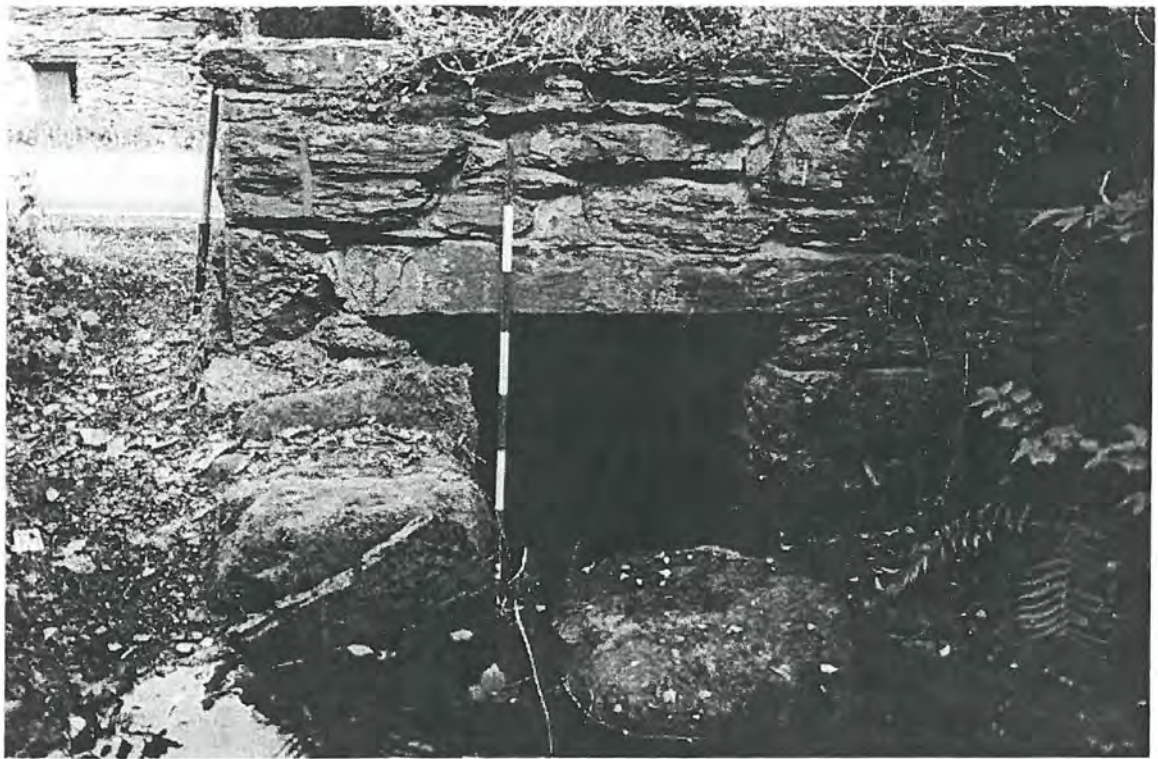


Plate 11. Site 93, culvert beneath the road

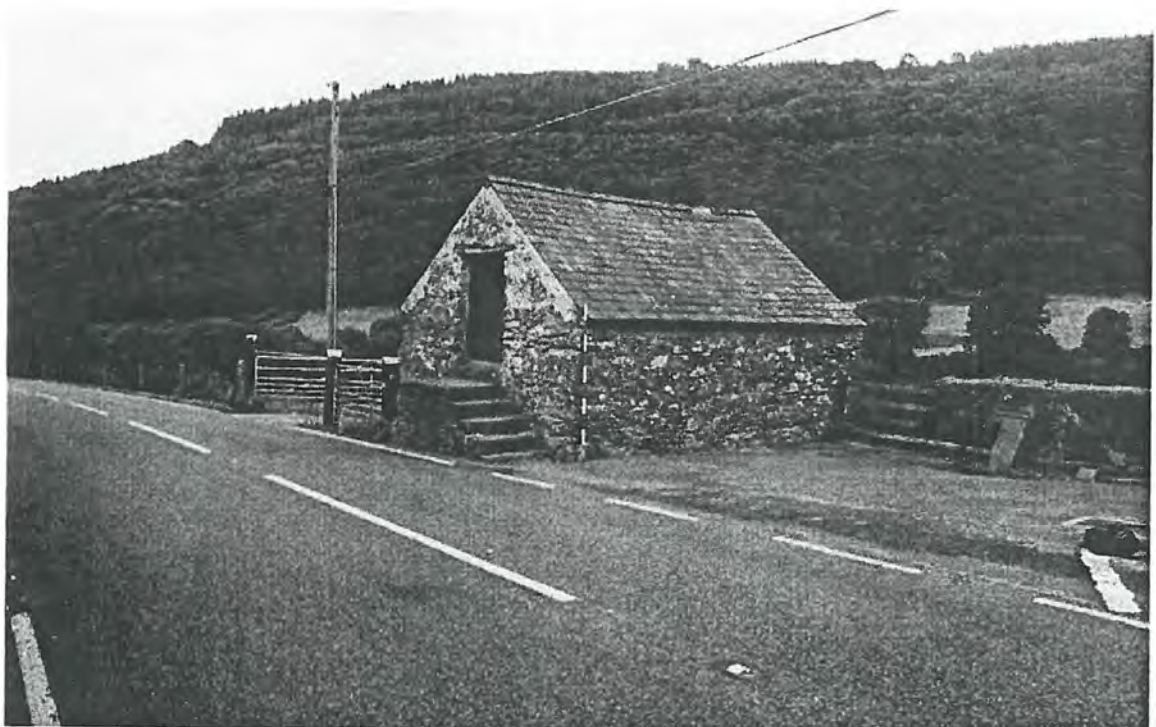


Plate 12. Site 94, building opposite Tan-yr-Allt



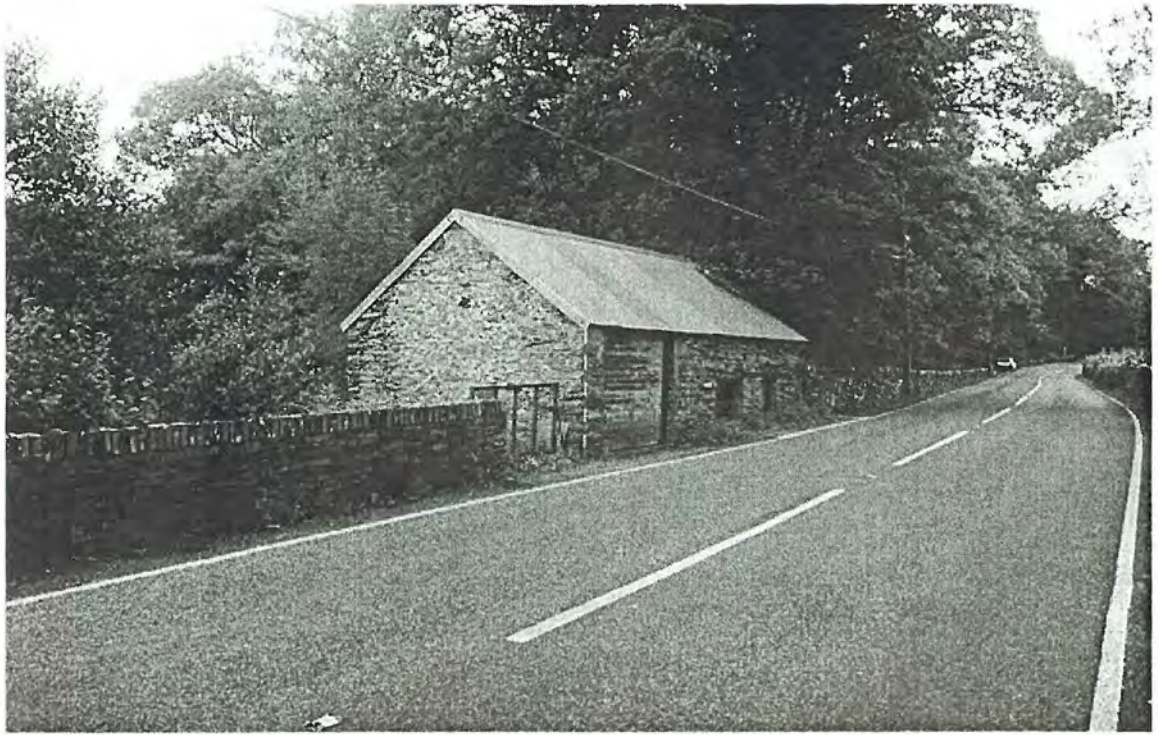


Plate 13. Site 94, building to the west of Tan-yr-Allt



Plate 14. Site 99, Pont-ar-Lledr, general view showing wing walls



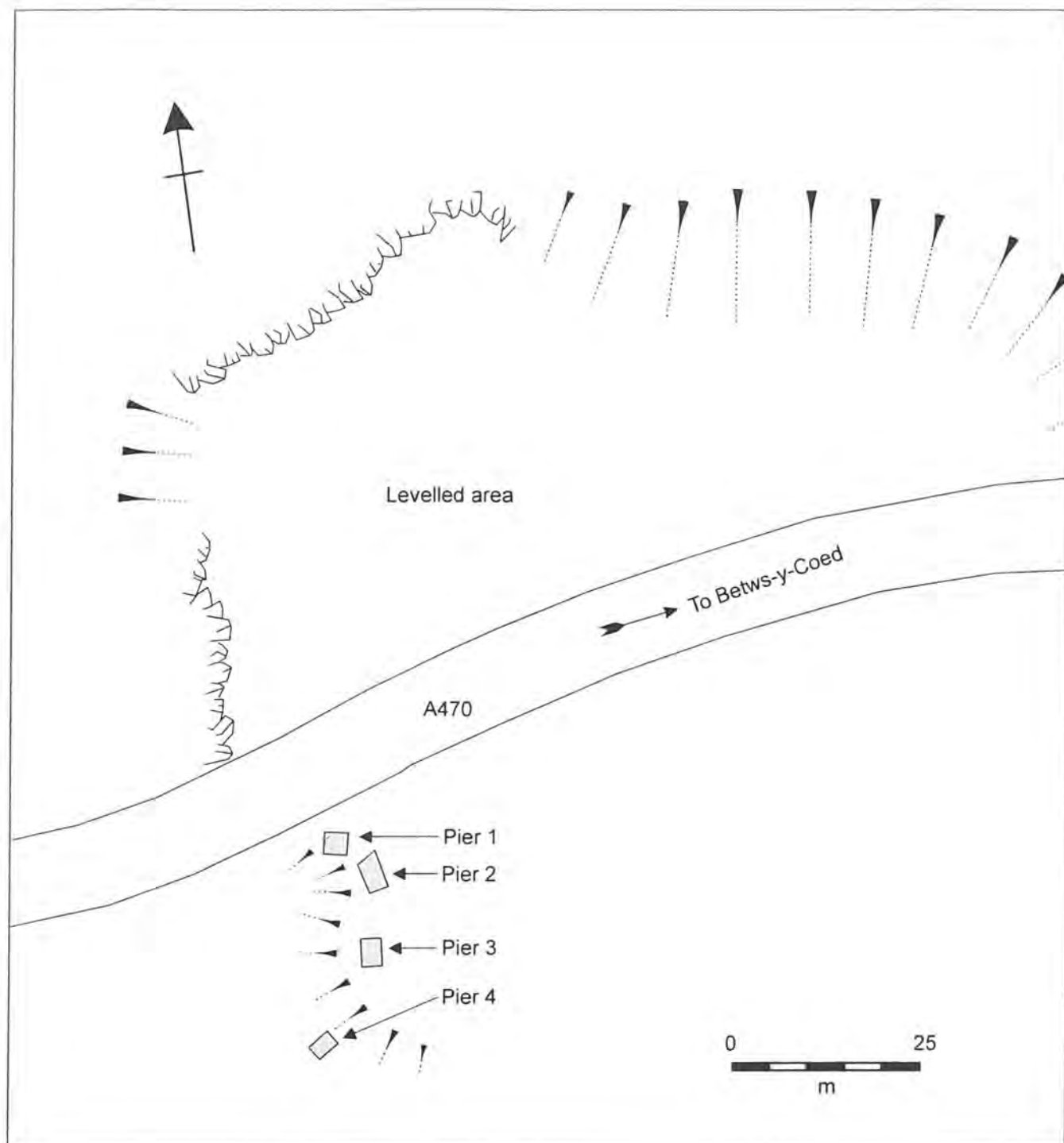


Fig. 8 Site 91, four stone piers



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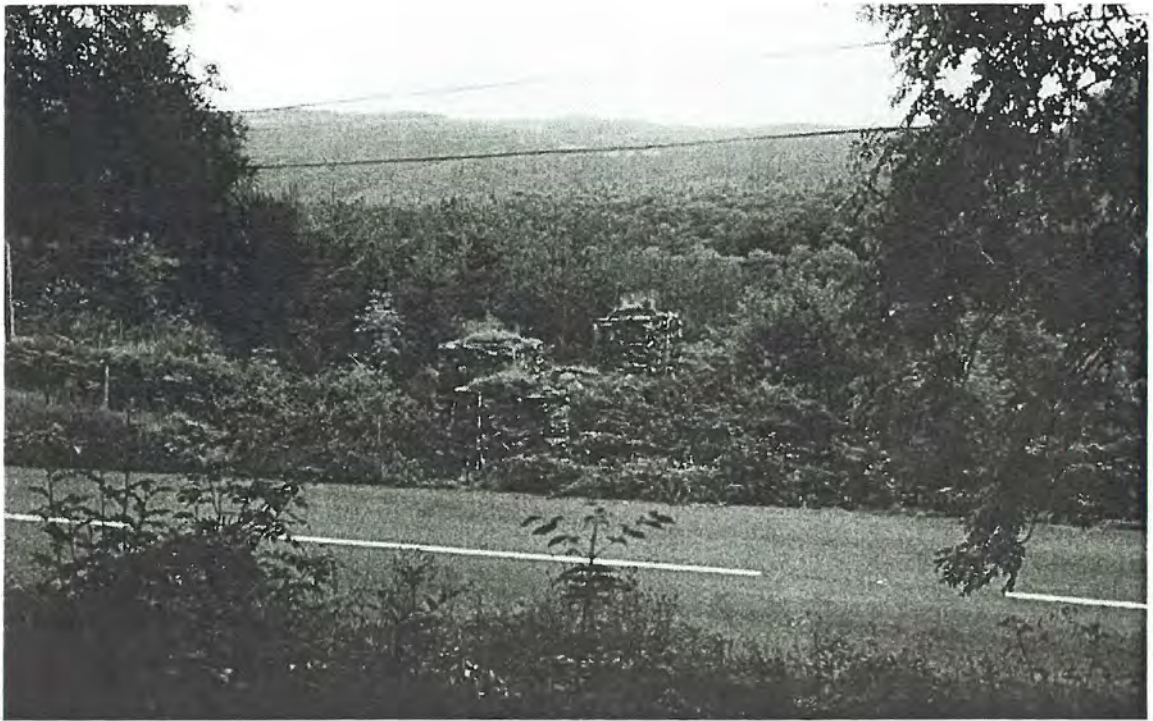


Plate 9. Site 91, piers 1 to 3



Plate10. Site 93, Culvert beneath the viaduct



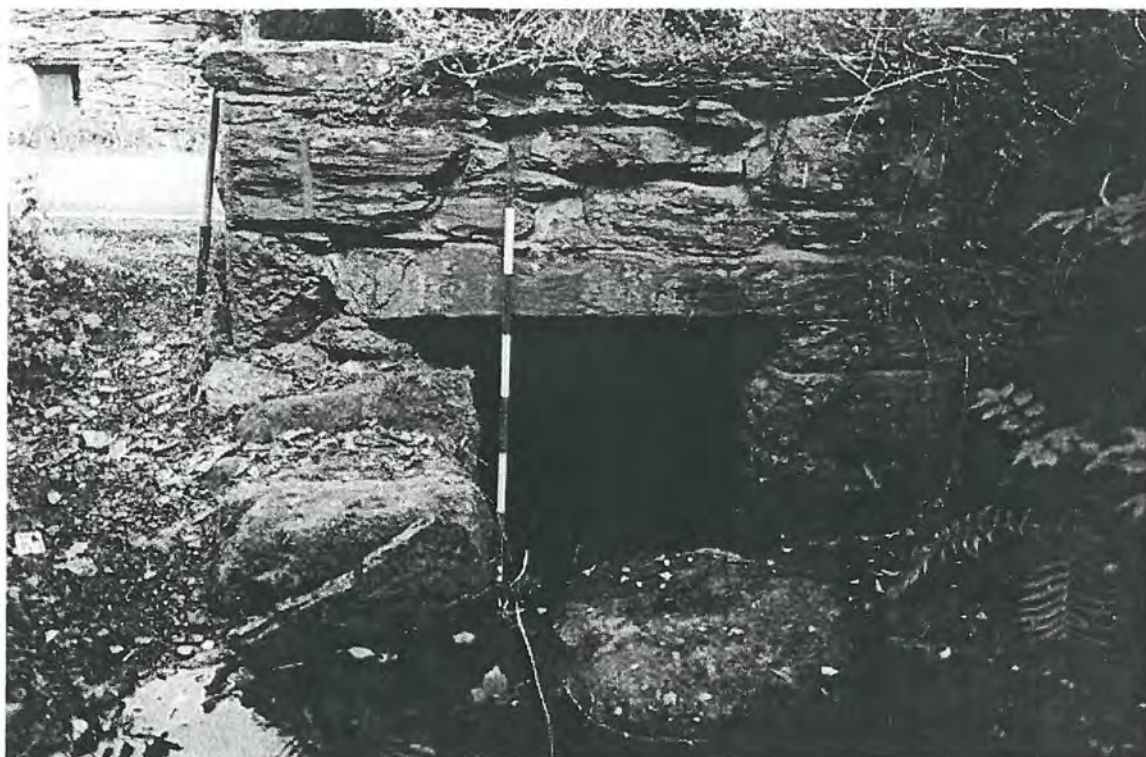


Plate 11. Site 93, culvert beneath the road

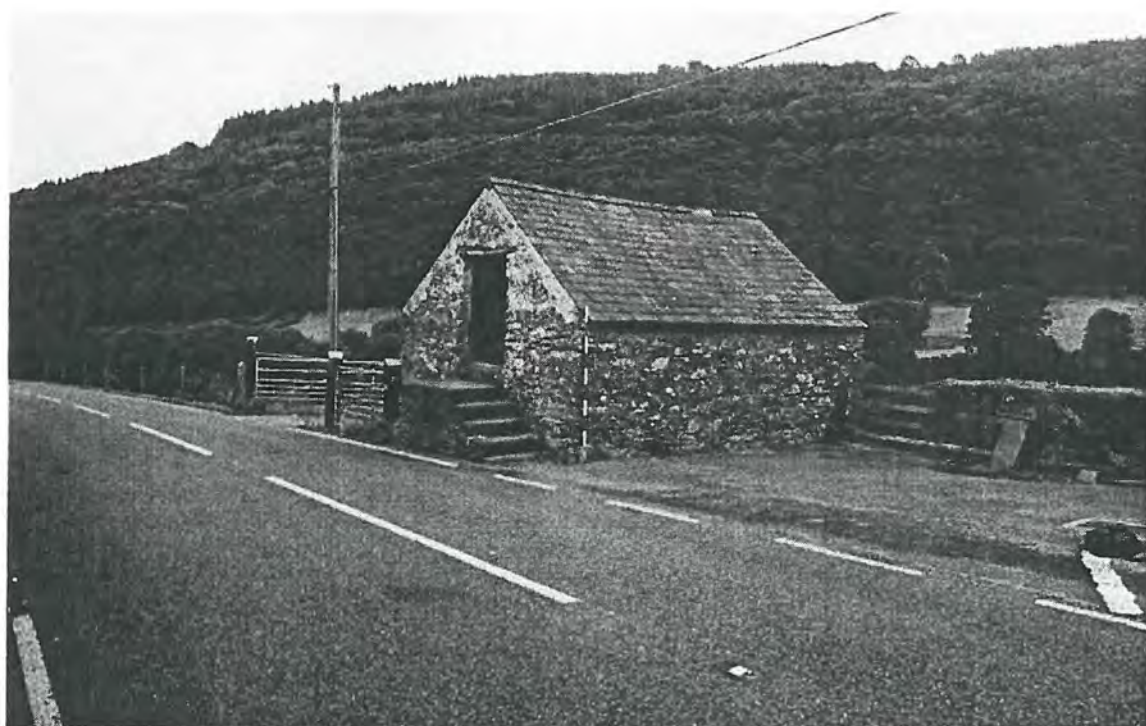


Plate 12. Site 94, building opposite Tan-yr-Allt





Plate 13. Site 94, building to the west of Tan-yr-Allt



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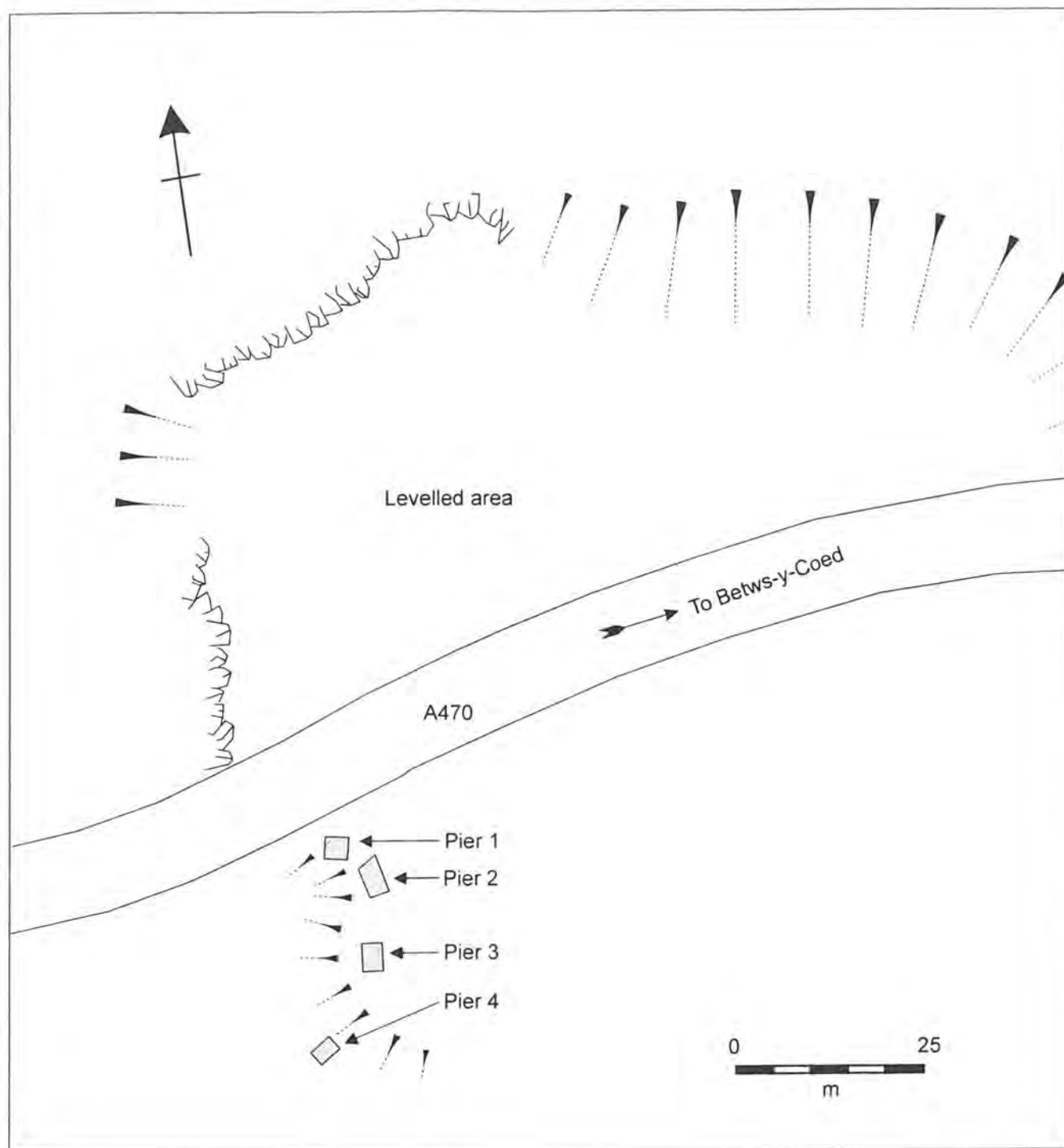


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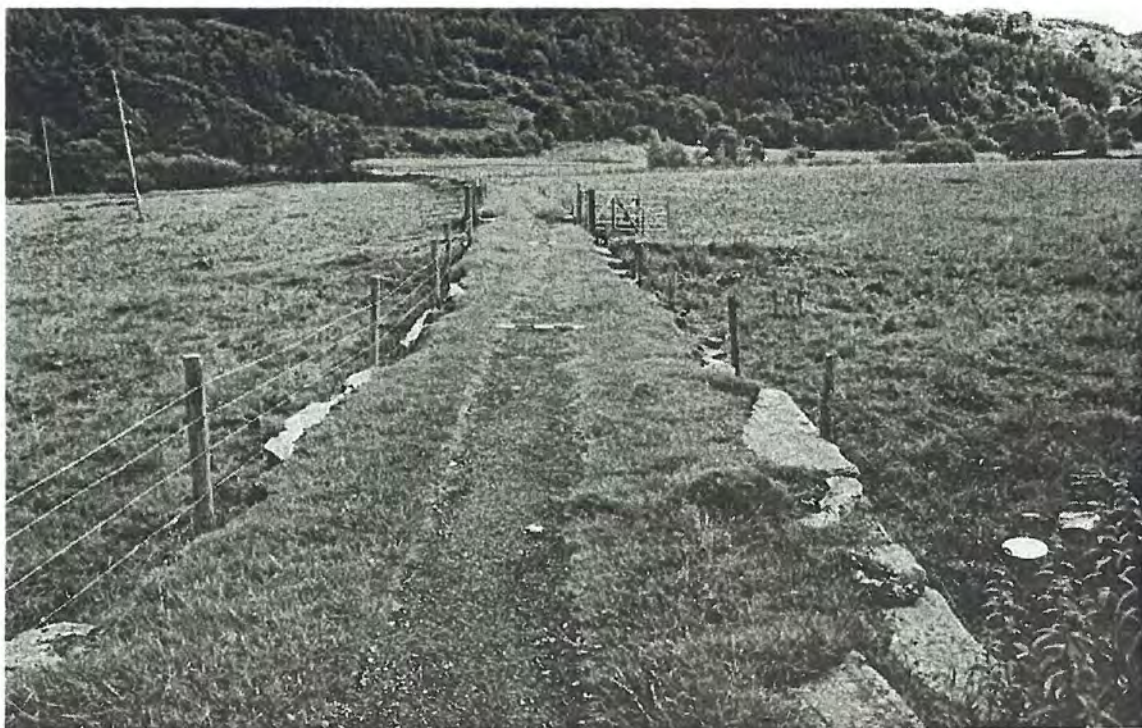


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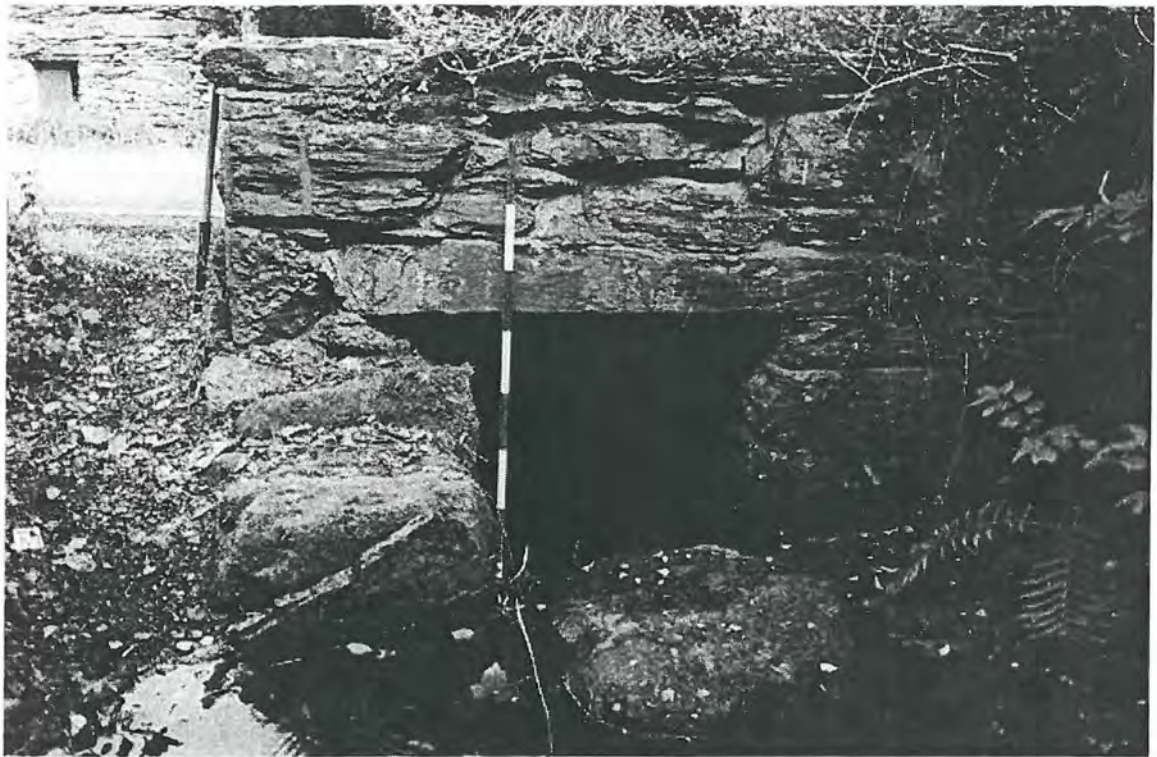


Plate 11. Site 93, culvert beneath the road

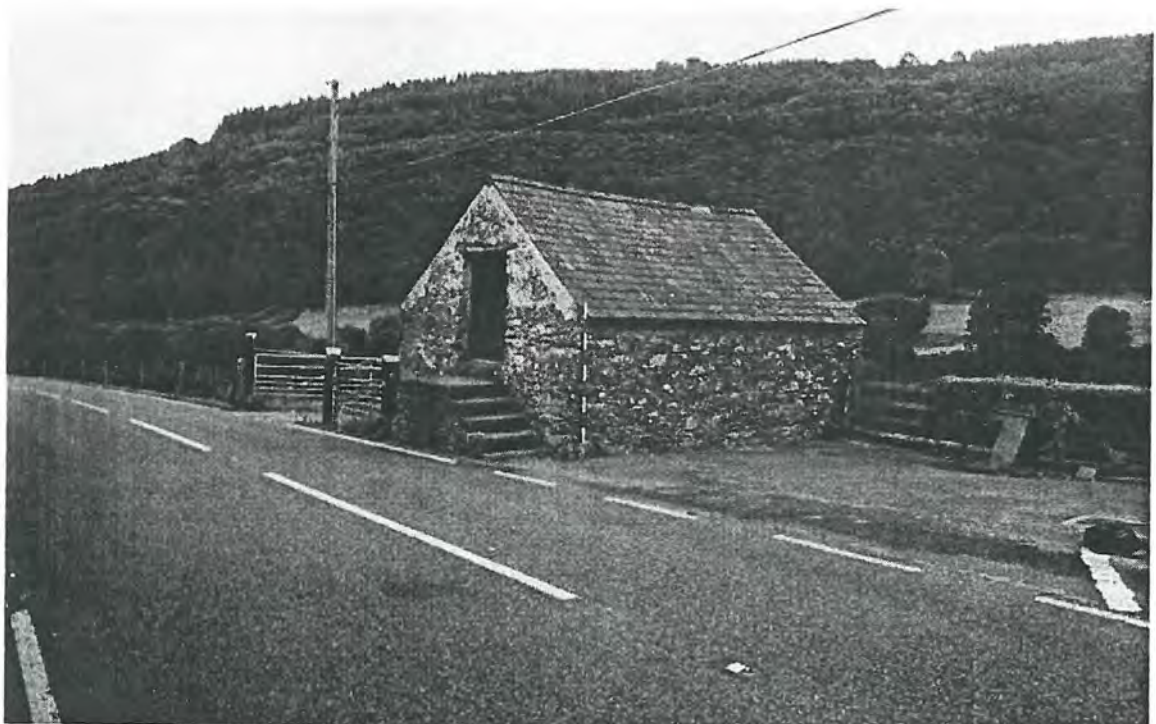


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Plate 14. Site 99, Pont-ar-Lledr, general view showing wing walls

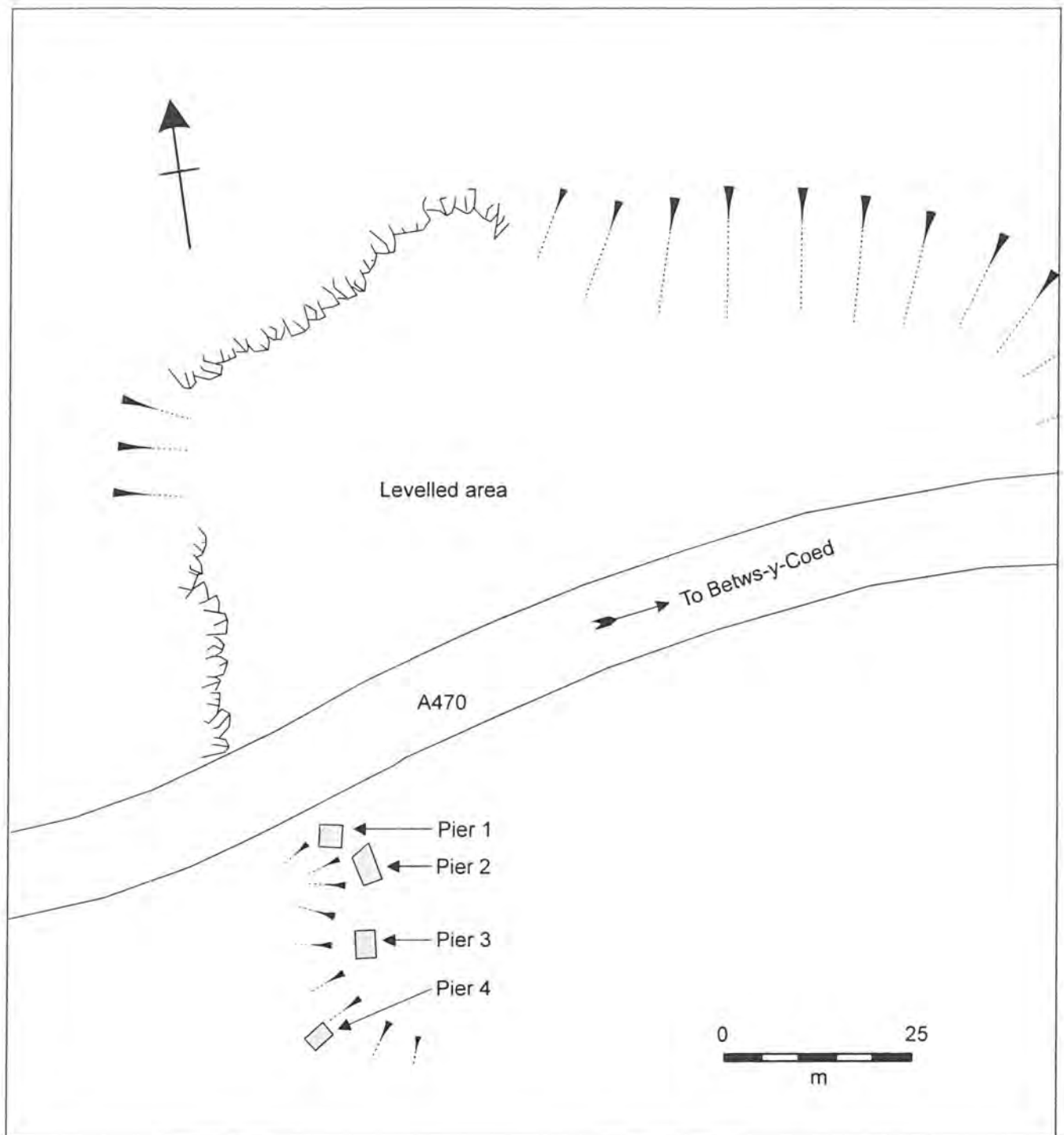


Fig. 8 Site 91, four stone piers





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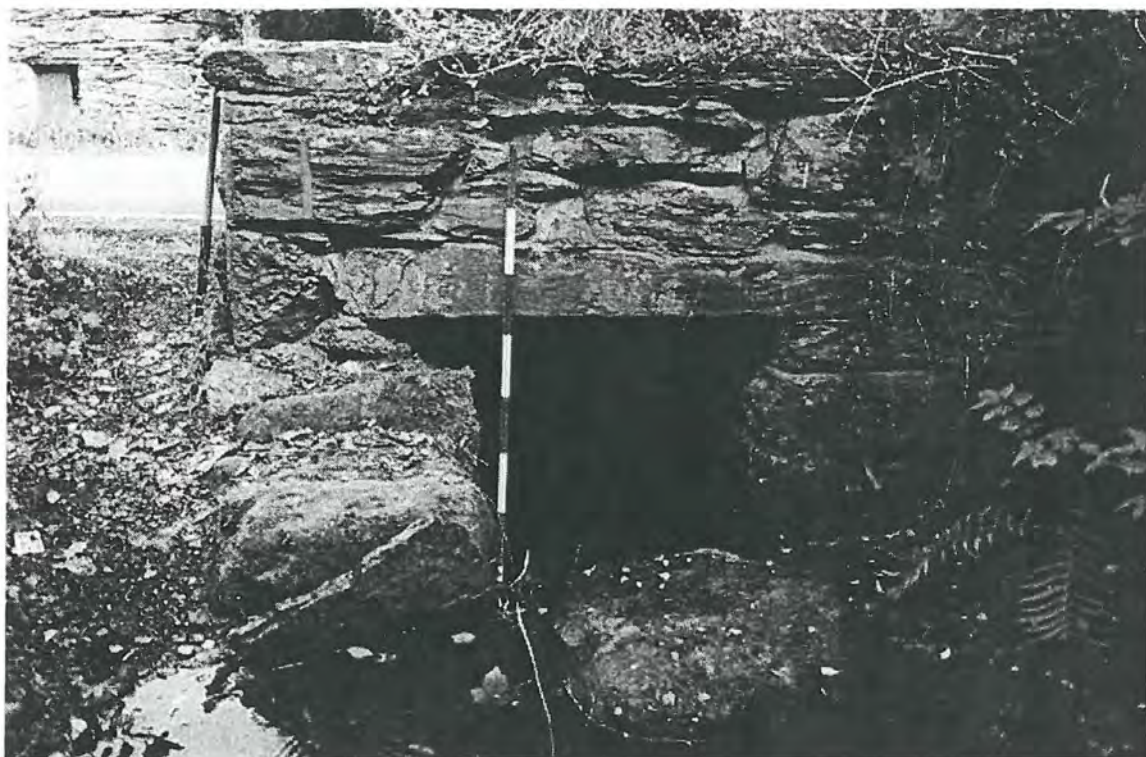


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