

**PEMBROKE DOCK INFRASTRUCTURE
PROJECT, THE DOCKYARD, PEMBROKE
DOCK, PEMBROKESHIRE**

HISTORIC BUILDING RECORDING

(ENCLOSING WALL – STRUCTURE NO. 2.5)



Prepared by DAT Archaeological Services
For: Milford Haven Port Authority



DYFED ARCHAEOLOGICAL TRUST

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By

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SUMMARY

DAT Archaeological Services were commissioned to undertake an archaeological scheme of historic building recording in association with the Pembroke Dock Infrastructure Project, at The Dockyard, Pembroke Dock, Pembrokeshire (SM 9579 0393). This report focuses on the recording of Structure 2.5 only, a masonry enclosure wall of the 1830s associated with the Captain Superintendent's House with the dockyard.

CRYNODEB

Comisiynwyd Gwasanaethau Archeolegol YAD i ymgymryd â chynllun archeolegol o gofnodi adeiladau hanesyddol ar y cyd â Phrosiect Seilwaith Doc Penfro, yn yr Iard Longau, Doc Penfro, Sir Benfro SM (9579 0393). Mae'r adroddiad hwn yn canolbwyntio ar gofnodi Adeiledd 2.5 yn unig, sef wal amgaeedig o waith maen o'r 1830au sy'n gysylltiedig â Thŷ'r Capten Uwcharolygydd â'r iard longau.

1. INTRODUCTION

1.1 Project Background

- 1.1.1 DAT Archaeological Services, in response to a request from RPS Group Ltd, on behalf of their clients the Milford Haven Port Authority (MHPA), were commissioned to undertake an archaeological scheme of historic building recording works in association with a scheme of works known as the Pembroke Dock Infrastructure Project, at The Dockyard, Pembroke Dock, Pembrokeshire (SM 9579 0393). Planning permission has been granted for the development (20/0732/PA), which is described as:

Demolition, part demolition and infill, modification of slipways, erection of buildings and ancillary development – for port related activities including the manufacture of marine energy devices, boat manufacture and repair and erection of plant.

- 1.1.2 A number of Conditions are attached to the outline planning permission. Condition No.15 states:

Development of each phase as approved by reason of condition 4 shall not commence until a programme of historic building recording for that phase has been submitted to, and approved in writing by, the local planning authority. The programme so approved shall be implemented in its entirety in accordance with the details so approved. The recording shall include details of how historic buildings and structures are to be recreated in digital format capable of being transferred into a Virtual Reality and/or Augmented Reality experience as part of the creation of a Virtual Dockyard as described in the Heritage Mitigation Strategy.

- 1.1.3 Conservation Area Consent has also been granted for the development (20/0901/CA). Condition No.4 of the Conservation Area Consent states:

Prior to the commencement of development, a programme of building recording and analysis shall be submitted to and agreed in writing by the Local Planning Authority. This shall be undertaken by a specialist acceptable to the Local Planning Authority in accordance with an agreed written brief and specifications.

- 1.1.4 A Written Scheme of Investigation (WSI) for a programme of building recording has been prepared by RPS Consulting Ltd on behalf of Milford Haven Port Authority (RPS 2021). This WSI has been approved by the local planning authority.

- 1.1.5 The WSI covers the site as a whole, which comprises a variety of different structures related to the history of the Dockyard from the early 19th century onward, a number of which are listed buildings. The development will be undertaken in a series of stages, and accordingly the WSI subdivides the buildings according to the different stages at which they will be affected by development. The development comprises Development Stages 1, 2A, 2B, 3 and 4. At present Development Stages 1 and 2A are in progress, or imminent. The area covered by these development stages include 11 individual structures.

- 1.1.6 The level of recording required for each structure varies, and is based around four different Levels, detailed in the WSI. The most basic recording, Level 1, includes post-Second World War buildings, Level 2 includes ancillary buildings constructed between 1926 and 1945, Level 3 includes other non-Listed buildings, and the highest level of recording, Level 4, includes Listed structures.

- 1.1.7 A full report will be produced based on the historic building recording of all structures included with Development Stages 1 and 2A. However, this report focusses on the historic building recording carried on the 'Enclosing Wall to the west of the former Captain Superintendent's House', Structure No.2.5. This structure is part of the Phase 2A Stage of development, with agreed recording to be carried out to Level 3. This recording is presented as a separate report at this stage to accompany an application for Listed Building Consent relating to proposed works on

this structure. These findings will be reproduced as part of the report into the remaining structures recorded during the Phase 1 and 2A Stage of works.

- 1.1.8 A number of site visits were made to record the various structures, including structure 2.5, between February and May 2022. Access to the exterior and interior of the enclosing wall was possible, although some sections of wall were heavily overgrown or obscured by modern ground disturbance.
- 1.1.9 The historic building recording has been undertaken in accordance with the Standard and Guidance for Archaeological investigation and recording of Standing Buildings and Structures (Chartered Institute for Archaeologists (CIfA), 1996, revised 2001 & 2008).

1.2 Site Location

- 1.2.1 Structure 2.5 lies within the walled dockyard at Pembroke Dock, located against the southern boundary wall, to the west of the former Captain Superintendent's House. The masonry wall encloses an area of rough, disturbed ground, covered in scrub and small trees. It is enclosed on its east side by a modern metal fenceline, beyond this lies the unoccupied Captain Superintendent's House and attached stable range. To the north lies a garage and truck parking adjacent to Whites Farm Way. To the west lies an area of rough ground adjacent to some large disused garages, the ground having been used as storage for excavated material and rubble.
- 1.2.2 Further west and north lie the enclosed dockyard area, including areas of maritime activity, municipal waste processing facilities and storage buildings. To the east lie early 19th century buildings along Admiralty Way and The Terrace. The dockyard occupies a square promontory on the southern edge of the Milford Haven Waterway. Slipways, quays and jetties line the northern side of the dockyard, a shingle beach lies to the west. The town of Pembroke Dock is largely laid out across level ground to the east, with some settlement to the south on rising ground.
- 1.2.3 The underlying geology of the area is limestone of the Pembroke Limestone Group. The rising ground to the south is formed of interbedded argillaceous rocks and sandstone of the Milford Haven Group, and to the north the limestone is mixed with mudstones and sandstones with and alongside the Haven (BGS 2022).

1.3 Scope of the Project

- 1.3.1 The building recording was designed to record the existing fabric of the structures (at the time of the survey) through a combination of a photographic survey, site notes, drawings and historical research.

1.4 Report Outline

- 1.4.1 This report describes the location of the site, its known archaeological and historical background and includes photographs of the site. Sources are referenced within the text or as footnotes.

1.5 Abbreviations

Regional Historic Environment Record – HER; Primary Record Number – PRN; National Grid Reference – NGR; Written Scheme of Investigation – WSI.

1.6 Illustrations

Record photographs are included to the rear of the text. Printed map extracts are not necessarily reproduced to their original scale and are illustrative only.



DAT



Figure 2: Detailed site layout of the Pembroke Dock dockyard. Building numbers are referenced in the WSI (RPS 2021). The location of enclosure wall, structure 2.5 is marked by the arrow. Plan produced by RPS, not reproduced to scale.

2 METHODOLOGY

- 2.1 The historic building recording scheme has been undertaken to a Level 3 standard as defined in the approved Written Scheme of Investigation (RPS 2021). This level of recording will comprise:

An introductory description followed by a systematic account of the building's origins, development and use. The record will include an account of the evidence on which the analysis has been based, allowing the validity of the record to be re-examined in detail. It will also include all drawn and photographic records that may be required to illustrate the building's appearance and structure and to support an historical analysis.

The information contained in the record will for the most part have been obtained through an examination of the building itself. The documentary sources used are likely to be those which are most readily accessible. The record will contain some discussion on the building's broader stylistic or historical context and importance and set each building in a broader historical and architectural discussion of the buildings as a group.

- 2.2 Based on the Level 3 record as defined in the Written Scheme of Investigation (RPS 2021) the following have been undertaken:

Written Record

- The building's precise location, as a National Grid reference and in address form.
- A note of any statutory designation (listing, scheduling or conservation area) and non-statutory designations (historic parks and gardens registers, local lists etc).
- The date of the record, the name(s) of the recorder(s) and, if an archive has been created, its location.
- A summary of the building's form, function, date and sequence of development, to include:
 - An account of the building's overall form (structure, materials, layout) and its successive phases of development, with supporting evidence.
 - An account of the building's past and present use, and the uses of its parts, with supporting evidence.
 - Any evidence for the former existence of demolished structures or removed plant associated with the building's use up to 1945.
 - An outline of the significance of the building.

Drawn Record

- Measured plans (to 1:50 level of detail).
- Measured elevations (to 1:50 level of detail).

Photographic Record

- A general view or views of the building (in its wider setting or landscape).
- The building's external appearance. Typically a series of oblique views will show all external elevations of the building, and give an overall impression of its size and shape.
- A record of each external elevation, taken at right-angles to the plane of the elevation.

- Any external or internal detail, structural or decorative, which is relevant to the building's design, development and use pre-1945, including any structural detail which contribute to an understanding of the purpose of the building.

3 HISTORICAL DEVELOPMENT

- 3.1 Detailed historical analysis has been undertaken by RPS and detailed in various documents, with the following brief outline extracted from the WSI (RPS 2021). Pembroke Dock was developed as a naval dockyard from the second decade of the 19th century, with the adjacent settlement of Pembroke Dock established at the same time. The dockyard was one of the largest shipbuilding yards in Great Britain, its period of naval use covering the period from wooden ships under sail, through to wooden steamships, ironclads and full steel vessels. The dockyard closed in 1926 but was then reused from 1931 by the Royal Air Force as a base for flying boats, eventually becoming the largest such base in the world, playing a crucial role during the Second World War. Part of the dockyard was also retained by the Admiralty during this period. The RAF vacated the site in 1959 and the small Admiralty yard closed in 2008. The major part of the former dockyard was vested in the Milford Haven Conservancy Board (not the Milford Haven Port Authority) upon its foundation in 1958. Since then the former dockyard has acquired new civilian users and tenants (RPS 2021).
- 3.2 The enclosure wall (Structure 2.5) is located in the southern part of the Dockyard area, and lies to the west of the extensive former Captain Superintendent's House. This house was initially planned as a house for the civilian Master of the yard in 1817-18, and nearby lodges, gatepiers and screen walls were constructed around this time to designs by Edward Holl. However, the house was not built until the Navy Board was combined with the Admiralty Board in 1832, when the first Captain Superintendent was appointed. A date of 1834 is also said to have been found on a removed wooden post within the house. The initial house was also designed by Edward Holl, but was subsequently built to a different design, although based on some of Holl's previous designs. The house was built of tooled squared limestone, three-storey with a basement. To the south lay an impressive stable range and service wing (Cadw – Listed Building description). It is assumed the enclosure contained by Structure 2.5 was laid out as a paddock associated with the extensive stables, delineating and separating the high status area of the Captain Superintendent's accommodation from the general dockyard workers, activities and town residents.
- 3.3 The wall is not however physically attached to the boundary of the Captain Superintendent's House, and elements of it have been suggested to pre-date the house. The southwest corner of the enclosure is attached to the main dockyard boundary wall. The dockyard wall, built of limestone rubble wall with ridged coping, was begun in 1813, but was not fully completed until the 1840s, with the line of the wall changing and rebuilt in various locations and at various times.
- 3.4 It is unclear when the section adjoining the enclosure wall 2.5 was constructed. Plans of the dockyard prior to the amalgamation of the Navy and Admiralty Boards in 1832 consistently show the southern line of the dockyard boundary running to the north of Paterchurch Tower (Figures 3 & 4), before turning south to join the current line along Fort Road. All plans of this era lack detail from within the dockyard to allow the line of this earlier wall to be traced with accuracy, but there is the potential for this southern return of the wall to follow the western boundary wall of Structure 2.5.

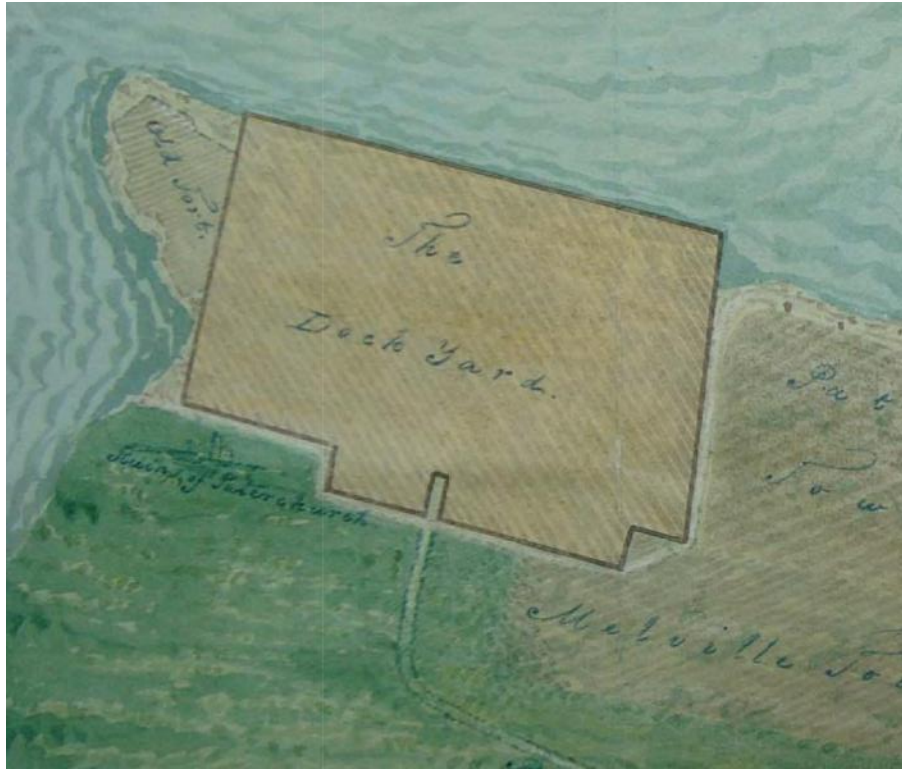


Figure 3: A sketch of Pembroke Head from 1824 (National Archives ADM 106/1969). It shows the southwest boundary of the dockyard running to the north of Paterchurch Tower before returning south to the current line along Fort Road.

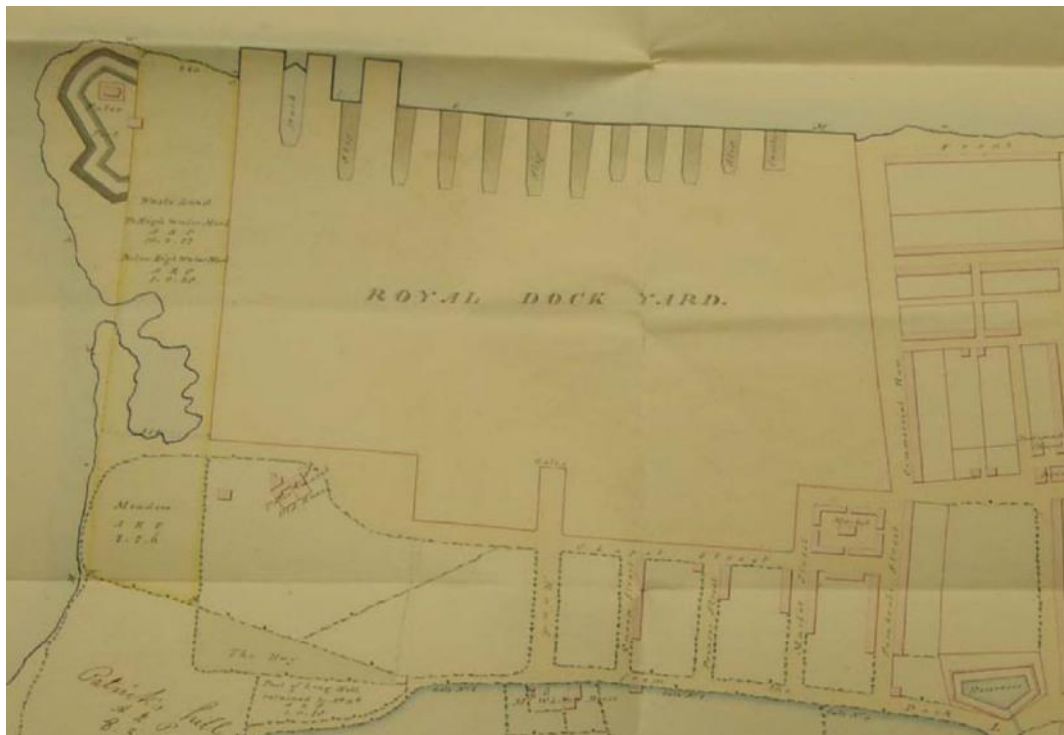


Figure 4: Plan of the Dockyard in 1830 (National Archives 106/1971), again showing the wall running to the north of Paterchurch Tower.

- 3.5 The current line of the dockyard wall, and the outline of the enclosure, are marked on plans subsequent to the development of the Captain Superintendent's house in 1832-4, as can be seen in figures 5 and 6.

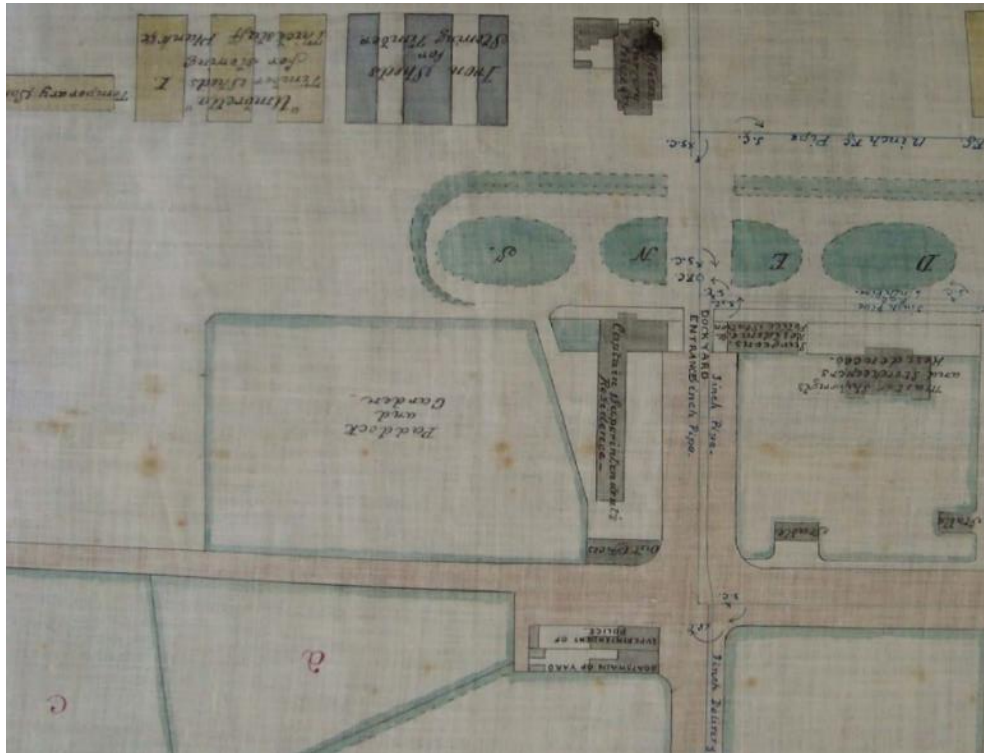


Figure 5: Undated plan showing location of water pipes (National Archives ADM 106/1970 AND 1971), produced after the construction of the Captain Superintendent's House, shows the enclosure outline and dockyard boundary wall in its present form. The enclosure is labelled as 'Paddock and garden'.

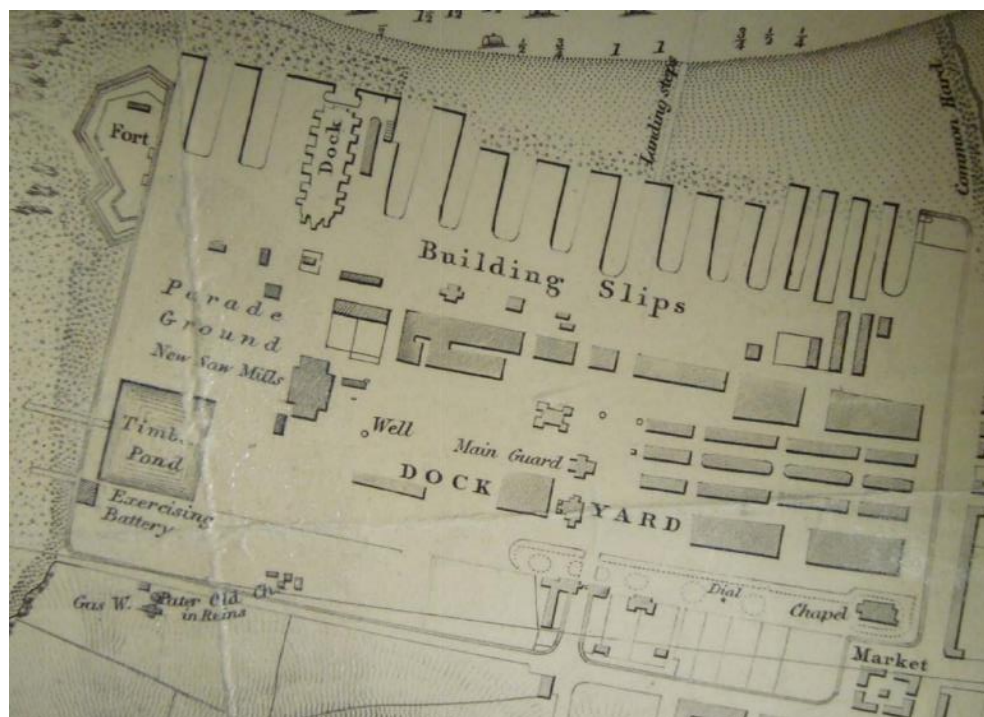


Figure 6: Plan dated to 1852 (National Archives MFQ 1/1286/11) showing the outline of the enclosure and the dock boundary wall in its current form.

- 3.6 The detail of the enclosure wall (Structure 2.5) is first shown on the 1:500 Ordnance Survey map of 1864 (Figures 7 & 8), which may provide some of the best historical evidence of the development of the structure. This map depicts the western enclosure wall as a continuous part of the dockyard boundary wall to the west. It suggests this formed a continuous line, running north for approximately 60m, terminating in a square pillar. The dockyard wall to the east is marked as butting against this wall. As the dockyard wall to the west appears to post-date 1830 (Figure 4), but the enclosure appears contemporary to the construction of the Captain Superintendent's House in 1832-4, a potential construction date for the western wall therefore appears likely to be contemporary to the house in 1832-4.
- 3.7 The northern wall of the enclosure is marked as a seemingly less substantial wall, but following the current line. The angled corner, and the final *circa* 8m at the northern end of the western wall, appears to be open however, the current line of the wall with the curved corner delineated as the edge of an open-sided drive that runs around the exterior side of the enclosure and dockyard boundary wall to the west (Figure 8). The northwest corner of the enclosure is instead enclosed by a small, squared enclosure extending into the main enclosure. The north wall appears to lie at the top of a bank, with a series of lampposts built along the southern edge of the wall, to light the main track that runs along the northern side. The east end of the wall ends with an angled return to the south, which continues to form the western side of an entrance accessing the stable range to the east.
- 3.8 The interior of the enclosure is subdivided by an east – west boundary through the centre, partly tree-lined. The southern half is potentially agricultural in nature, with a hay yard enclosure and cow house range in the southeast corner. The function, and indeed access to the northern half is unclear. A fall in ground level is marked along the northern edge, along the internal edge of the northern wall. Formal drives and garden areas lie to the northeast, possibly part of the Edward Holl designs of the 1810s, the remaining areas to the immediate north and west of the enclosure appear undeveloped.

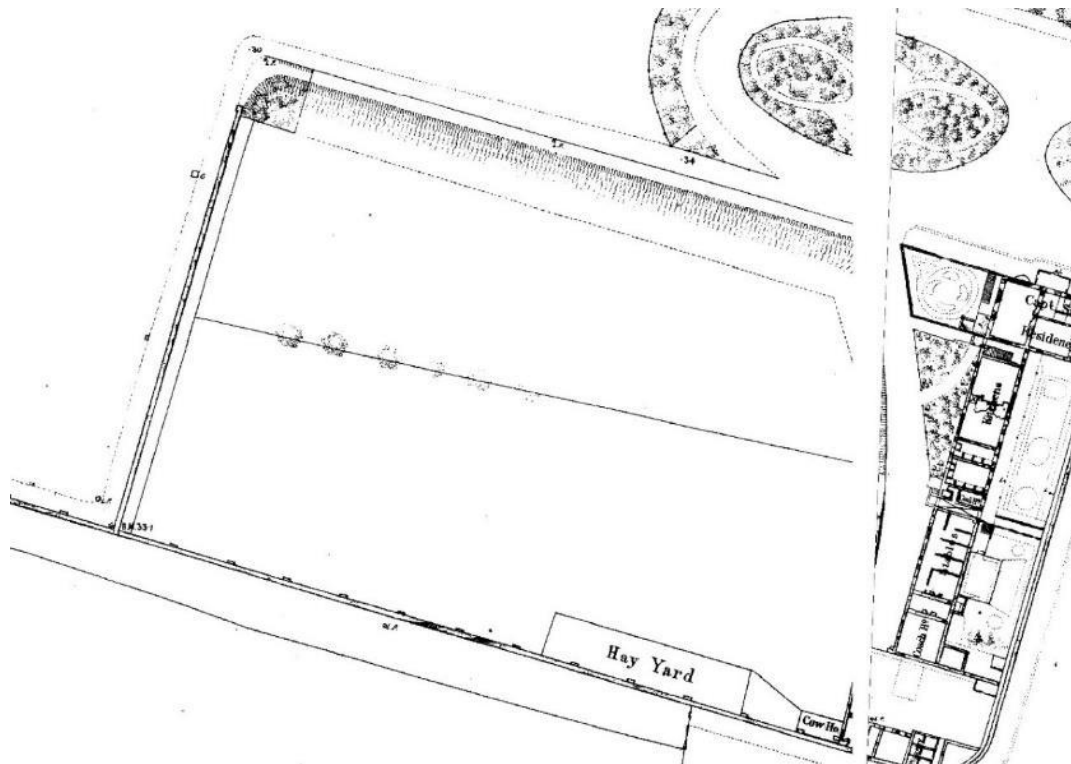


Figure 7: Extract from the detailed Ordnance Survey map of 1864.

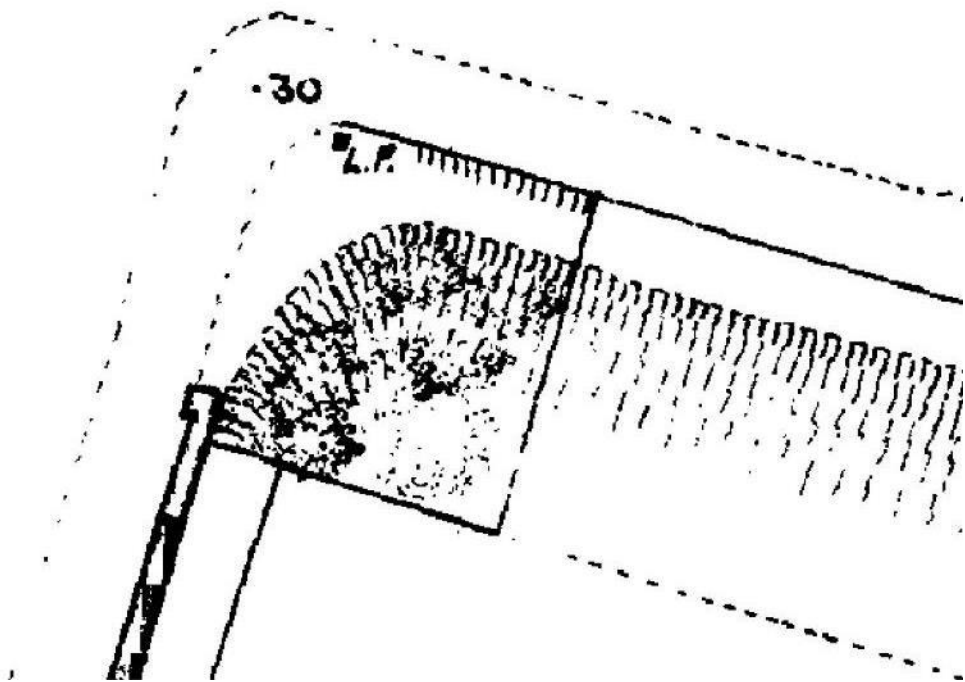


Figure 8: Detail of the northwest corner of the enclosure from the 1864 Ordnance Survey map.

- 3.9 Subsequent mapping is less detailed, and the enclosure wall is marked as a continuous feature. The subsequent 1:2500 Ordnance Survey map of 1895 essentially appears unchanged, but the interior appears to have been amalgamated into a single enclosure, with an unusual irregular enclosure occupying the northeast corner, containing a series of linear and irregular banks or ponds. An Admiralty Plan of 1901, and a subsequent Admiralty Record of 1909 continue to show the square enclosure in the northwest corner, along with a large greenhouse in the northern part of the enclosure. Detailed plans of the 1930s and 1940s show a tennis court established in the western part of the enclosure, but lack any further detail on the enclosure walling.
- 3.10 One plan of the dockyard from the later 1930s suggests the land to the west of the enclosure also formed part of the grounds associated with the Captain Superintendent's House and grounds, and an aerial photograph from 1930 and 1931 (Figure 9) depicts this area as similarly undeveloped, with a garden or parkland appearance surrounded by mature trees, suggesting a more direct link between the enclosure and the lands to the west by this time. The enclosure itself appears to be laid out as gardens at this time.

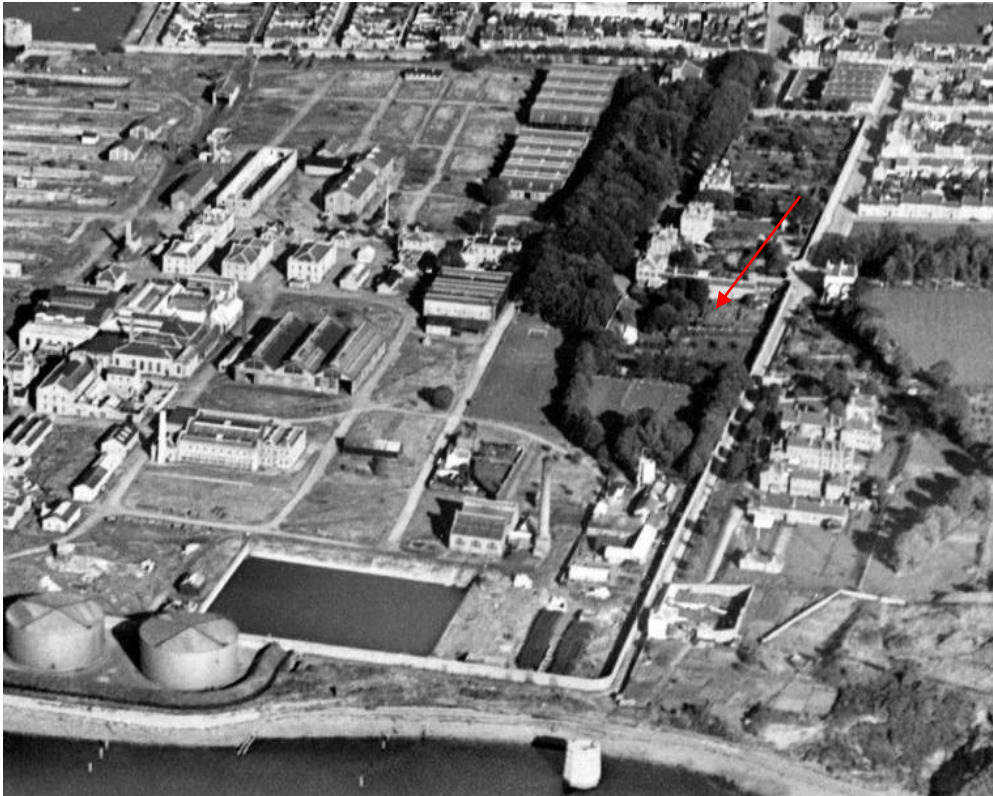


Figure 9: Aerial photograph from 1931. The enclosure is marked by the arrow. (R J Thomas collection).

- 3.11 Aerial photography from the 1940s (Figure 10) show the enclosure laid out with what appears to be delineated areas of garden and mature trees, and a tennis court laid out towards the western end of the enclosure. The main entrance to the grounds would appear to be via a splayed driveway on the west side of the Captain Superintendent's House, with what appears to be a gateway onto a small courtyard (all currently lying to the east of present accessible enclosure area). The boundary is lined by mature trees, with a stand of trees to the exterior of the eastern half of the northern boundary, and a building complex to the exterior of the western half. The grounds to the west of the enclosure are open.



Figure 10: Aerial photograph taken by the RAF in 1946 (106G UK 1625 6351), the enclosure is marked by the arrow.

- 3.12 Aerial photographs of the 1950s suggest the enclosure had been subdivided into numerous garden units, potentially representing an allotment. The next series of aerial images examined, dated 1978, indicate the enclosure had been abandoned, with mature trees spreading throughout. The current modern eastern boundary had also been established by this time, with the original eastern part of the enclosure, where it adjoins the Captain Superintendent's house complex, redeveloped into an open area with an open drive from the main road to the north, accessing the southern end of the former stable range. The garages to the west of the enclosure had also been constructed around this period. By 1983 the garage to the north had been added, and the breach in the northern wall is apparent in photography from 1987.

4 STRUCTURAL DESCRIPTION (Figures 11 – 14, Photos 1 – 25)

- 4.1 The enclosure wall to the west of the former Captain Superintendent's House (Structure No.2.5), also referred to as the 'Paddock Wall', is a single enclosure wall, L-shaped in plan enclosing an area of ground approximately 0.7 hectares (1.7 acres) in size. To the southwest the wall adjoins the main boundary wall of The Dockyard, to the northeast the wall adjoins the immediate boundary to the former Captain Superintendent's House (later The Port Hotel).
- 4.2 The wall runs for 70m from the Dockyard boundary wall, in a north-northeast direction, before turning a right -angle, with a rounded corner, to run 76m in a east-southeast direction as far as a modern boundary, although the wall itself appears to continue for another *circa* 12m beyond that point (Figure 11, Photo 1).



Photo 1: View southeast of the west side of the enclosure wall, adjoining the Dockyard wall to the rear.



Figure 11: Plan of the enclosure wall, Structure 2.5, highlighted in red.

- 4.3 The main Dockyard boundary wall, as it runs along Fort Road, is approximately 3.5m high, built of squared and roughly faced limestone rubble with squared coping stones that project slightly, topped by smaller stones set in a mortar ridge (Photo 2). A series of squared internal buttresses are spaced at regular intervals.



Photo 2: South facing shot of the internal face of the main Dockyard boundary wall.

- 4.4 At the point the enclosure wall adjoins the Dockyard boundary wall the join is partly obscured, but may be at least partially tied in. At this point the enclosure wall is built in the same style as the Dockyard boundary wall, with squared and roughly dressed limestone rubble, topped by ridged coping that runs continuously from the Dockyard boundary wall. The wall is initially approximately 3.5m high but soon (after 2m) drops down to a height of approximately 2.5m, with the dressed stone coping following a curved line to this lower level (Photo 3).
- 4.5 The enclosure wall then continues at this height for 60m. The 'exterior' west face of the wall appears consistent throughout this length. The stonework is placed in series of random courses, alternating larger and smaller stonework, set within a light cream lime mortar, occasionally set with abundant red sandstone inclusions (Photos 4 & 5).
- 4.6 The foundations of the wall are exposed roughly midway along this length, adjacent to a manhole cover. The wall is sat on stepped foundations, the initial step 0.3m deep and protruding approximately 0.1m, then a lower step at least 0.6m deep, protruding approximately 0.15m. A later upright iron stench pipe has been attached to the face of the wall at this point, likely associated with the adjacent manhole cover (Photo 6).
- 4.7 This section of the wall terminates at an asymmetrical masonry pillar, forming a narrow, squared appearance from the exterior side, wider to the interior (Photos 9 – 11). The pillar is 0.6m wide on the western face (protruding 0.15m), 0.8m wide on the eastern face (protruding 0.3m), 3.8m high. It is built of roughly dressed but neatly squared limestone blocks. It is topped flat coping stones, capped with a pointed coping of smaller stones set in mortar.

The adjoining coping on the wall to the south appears to have been repointed, or replaced in a similar style.

- 4.8 The 'interior' east face of this section of wall is largely ivy covered and fronted by small trees, but where visible it demonstrates a consistent building style (Photos 7 & 8). A series of four regularly spaced internal buttresses are recorded towards the northern half of this stretch of wall, but largely hidden by undergrowth, and further buttresses may exist to the south. The pillar at the north end forms a fifth buttress. The internal face of the wall appears to butt against the larger stonework of the pillar.



Photo 3: West facing shot of the point at which the enclosure wall adjoins the main Dockyard boundary wall.



Photo 4: Southeast facing shot of the enclosure wall. 2m scale.



Photo 5: East facing shot of the enclosure wall. 2m scale.



Photo 6: East facing shot of the attached iron stench pipe, with exposed foundations adjacent. 2m scale.



Photo 7: West facing shot of the interior of the enclosure wall.



*Photo 8: West facing shot showing detail of the interior face of the enclosure wall.
2m scale.*



Photo 9: East facing shot showing the pillar at the end of the north – south stretch of the enclosure wall. 2m scale.



Photo 10: West facing shot of the interior of the pillar. 2m scale.



Photo 11: Detail of the pillar. 2m scale.

- 4.9 To the north of the pillar the wall height increases to approximately 3.25m and continues to form the curved northwest corner of the enclosure. The stonework and building style remain similar on the external face, but a change in mortar suggests much of this wall may be a rebuild or repair, although the line of coping stones are tied in to the pillar (Photo 12). The internal face shows a change to a rougher style of smaller stonework, and a clearer butt joint between the pillar and wall (Photo 16). The coping itself continues in the style of large squared stone topped by smaller stones set in mortar, but is no longer ridged, instead angled down from the internal face to the external face (Photo 14). The coping is placed on a layer of slate, which overhangs on the internal side around the curving corner.
- 4.10 4.1m to the north of the pillar lies a doorway, 1.2m wide, 2.1m high, with a squared dressed stone lintel, and squared edging (Photo 12 & 17). Opposing metal brackets set into the masonry surround, with concrete mortar adjacent, suggests the external face of the opening has been blocked at some point. Missing stonework below the stone lintel may be part of this former blocking. No original gate or door fixings are apparent in the door surround.
- 4.11 As the wall turns to run east-southeast the height of the wall drops to 1.2m on the exterior (north) face, 1.75m on the internal (south) face, with the coping curving down to the new height, mirroring the initial change in height close to the Dockyard Wall (Photo 15 & 18).



Photo 12: East facing shot of the curving northwest corner, with doorway. 2m scale.



Photo 13: Southeast facing shot of the curving northwest corner. 2m scale.



Photo 14: Southeast facing showing detail of the damaged coping on the northwest corner.



Photo 15: South facing shot showing the eastward continuation of the enclosure wall from the northwest corner. 1m scale.



Photo 16: Southwest facing shot showing the butt joint between the corner wall and the pillar, visible to the interior. 2m scale.



Photo 17: North facing shot showing detail of the frame of the opening in the northwest corner. 2m scale.



Photo 18: Northeast facing shot showing the continuation of walling east from the corner. 1m scale.

- 4.12 Approximately 13m along the wall an irregular hole lies in the southern wall face and extends through the width of the wall. The hole is *circa* 0.4m by 0.2m in size, the irregular nature suggesting it may have been caused by the removal of an inbuilt attachment, or substantial vegetation disturbance. In line with the top of the hole, set 1m to the west, a short, rounded metal rod has been mortared into the wall (Photos 19 & 20). No further evidence of structural attachments or changes in the stonework are noted in this area.
- 4.13 The northern wall of the enclosure continues from the corner for approximately 33m to a blocked doorway (Photos 19 – 22). The doorway is 1.5m wide, flanked by dressed limestone quoins, infilled by concrete breezeblock. The upper stones around the door have gone (Photo 23). A further 2.4m beyond the door an 8.3m wide section of the wall has been removed (Photo 22 & 24).
- 4.13 Beyond this the wall continues, at a height of 2m, for a further 28.5m (Photos 25 & 26) before further access is blocked by a modern metal fencing marking the current boundary to the former Captain Superintendent's House.



Photo 19: North facing shot of the internal face of the northern enclosure wall. The hole in the wall is just visible to right. 1m scale.



Photo 20: North facing shot showing the hole in the wall, with protruding metal bar to the left.



Photo 21: North facing shot of the internal face of the northern enclosure wall. 1m scale.



Photo 22: Northeast facing shot showing the removed section of the enclosure wall.



Photo 23: North facing shot of the blocked doorway in the northern enclosure wall. 1m scale.

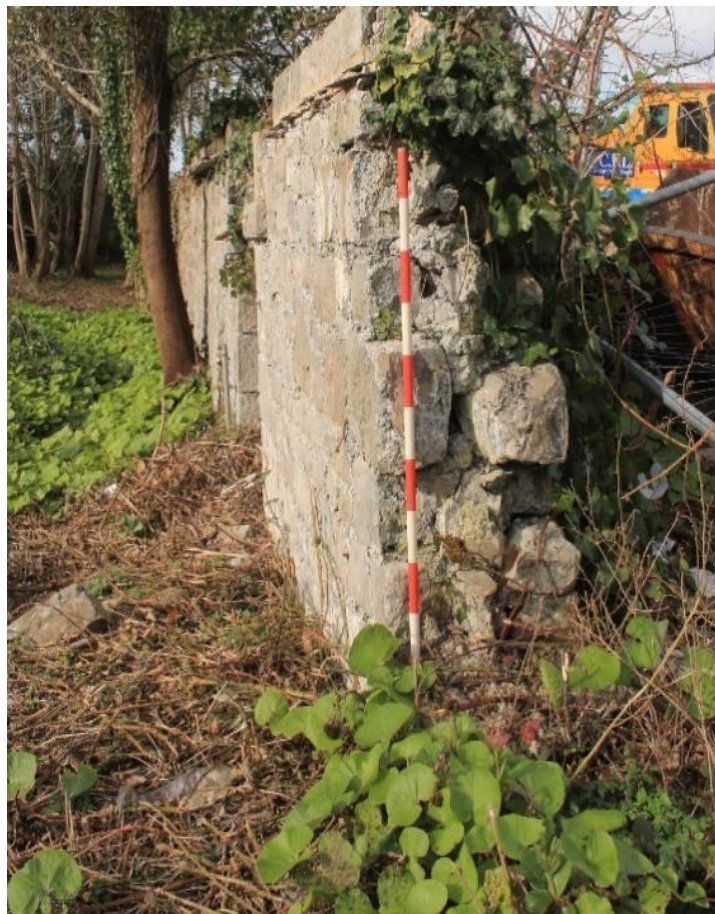


Photo 24: West facing shot of the edge of the removed section of wall, detailing the wall make-up. 1m scale.



Photo 25: Northeast facing shot of the eastern end of the northern enclosure wall. 1m scale.



Photo 26: North facing shot of the eastern end of the northern enclosure wall. 1m scale.

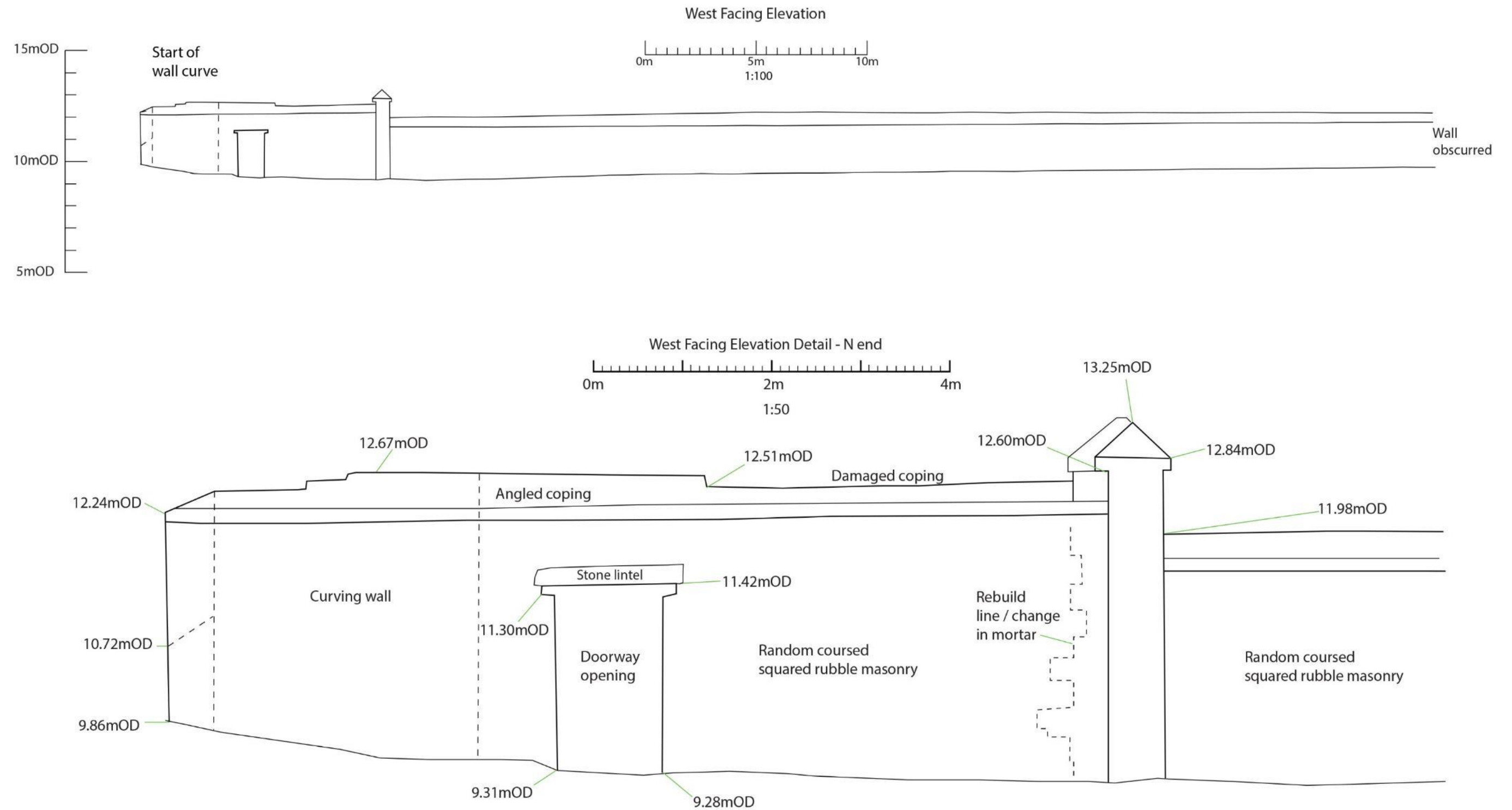


Figure 12: West facing elevation of Structure 2.5. Original survey produced by Merrett Surveys, adapted and annotated by DAT-AS.

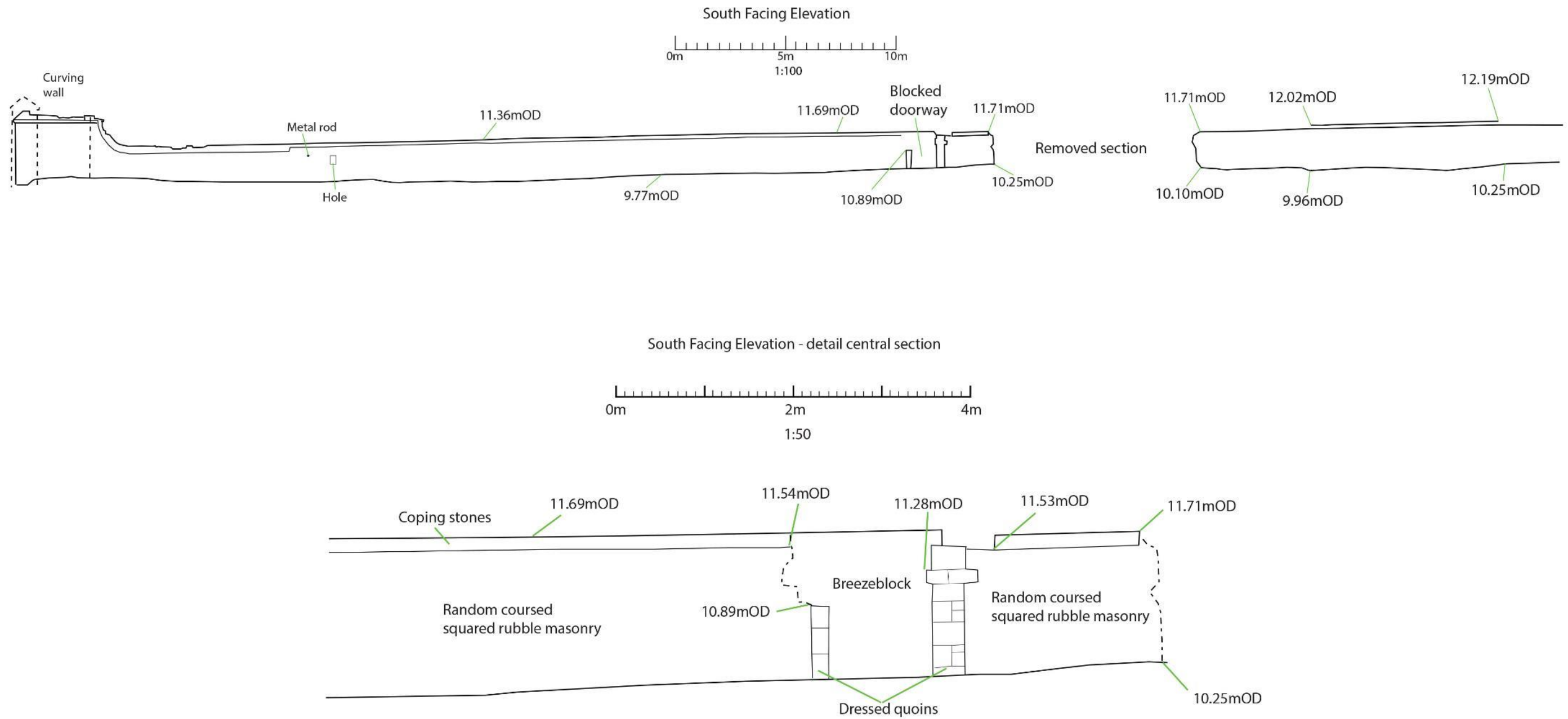


Figure 13: South facing elevation of Structure 2.5. Original survey produced by Merrett Surveys, adapted and annotated by DAT-AS.

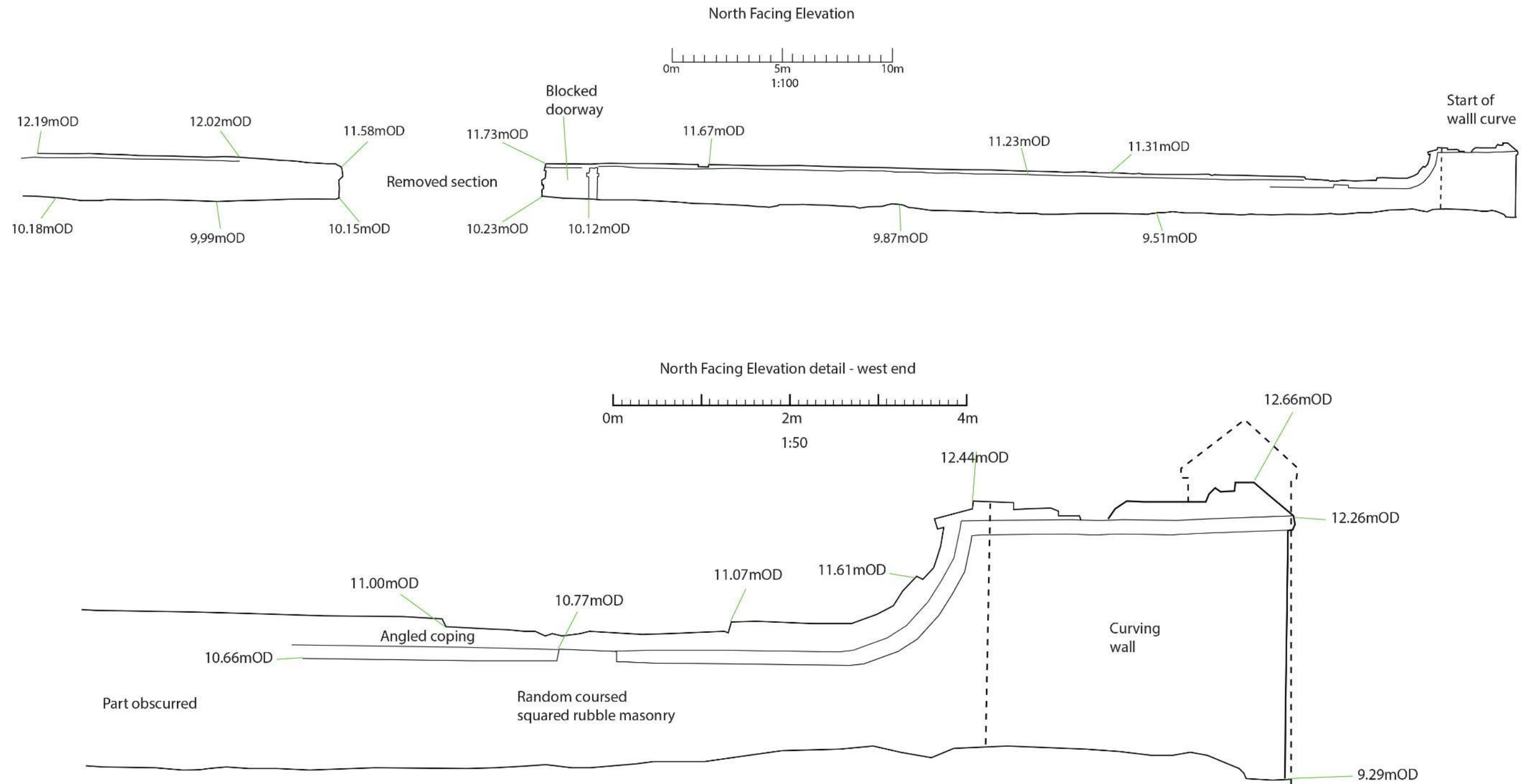


Figure 14: North facing elevation of Structure 2.5. Original survey produced by Merrett Surveys, adapted and annotated by DAT-AS.

5 CONCLUSION

- 5.1 Structure 2.5 is a masonry boundary wall, built in a style to mirror the main dockyard boundary wall, and enclose an area associated with a high-status dwelling. The wall enclosed an area of semi agricultural land, grazing, gardens and leisure activity. The wall is largely continuous, but with two doorways to give access through the wall, potentially both added at a later date (see below). One doorway lies in the northwest corner, allowing access to further grounds associated with the Captain Superintendent's House, with a second doorway added in the northern wall. A small hole and metal bar in the northern wall may be indicators of a former internal garden structure, although no further structural evidence on the walling is apparent, and no structure is marked on historic mapping or photographs.
- 5.2 Evidence suggests the western wall of the enclosure was constructed first, likely contemporary to the construction of the Captain Superintendent's House in 1832-4, at which point the main dockyard boundary wall to the west may also have been (re)built. The northwest corner and northern boundary wall appear to be of one build, but added later, albeit following the original northern boundary of the enclosure. This section may have been added subsequent to the Ordnance Survey map of 1864 which suggests the northwest corner was open, and added the access points into the enclosure from the west and north. Care was clearly taken in this construction to continue the original building style of the western wall.
- 5.3 By the mid-20th century the enclosure may no longer have been directly associated with a high-status dwelling, potentially in use as allotments. The enclosure appears to have been abandoned altogether by the late 1970s, with the loss of some of the northern wall in the late 1980s.

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